

avenue of flags urban design proposal

buellton california



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In the 2012 Spring Quarter, Cal Poly's City and Regional Planning Urban Design Studio II students collaborated with the City of Buellton to develop urban design proposals for the Avenue of Flags. The city wished to enhance community life through renovation of the site into a downtown. The Urban Design students formed four teams and they collaborated to create a design proposal. The teams visited the City of Buellton to hold various community meetings in which they received feedback from community members regarding what they wish to see achieved on the Avenue of Flags.

The first Community Meeting was held in order to let residents express their likes and dislikes of the Avenue of Flags to give students an understanding of where to begin the design process. The residents were given "have" and "wish" poems to answer which allowed them to state what they enjoyed about the Avenue, as well as what they wished to see enhanced upon. Once the students were finished gathering information from community members, the teams joined to residents on a tour of the Avenue to gain a better understanding of the project site. While walking along the Avenue, students talked with residents to gain more specific information on certain parts of the Avenue and ideas community members had for it. The teams took all of the information gathered from this first meeting, and then

developed conceptual diagrams with their first proposal for the site.

At the second Community Meeting held in April, the teams brought their conceptual diagrams to show community members what they were envisioning for Avenue of Flags, and received feedback from residents. Each team presented their design proposal to the residents and allowed them to draw and write on the proposals if they had suggestions. In addition, the teams had community members draw on a map of Avenue of Flags to design what they envisioned as the perfect design. From this, the teams decided upon plan objectives that they saw most crucial to base their design proposals off of.

At the third and fourth community meetings, each team brought their proposed land use maps and illustrative site plans they developed after receiving feedback from community members. These studies illustrated what the teams hoped to achieve in their design proposals, which allowed the community to get a sense of what the Avenue could potentially look like in the future. The community members rotated between teams,

offering constructive criticism on each proposal. This was the final meeting before the teams presented their urban design proposals.

On June 6, 2012, the teams met with the Buellton community, as well as the city council members and the planning commission to present their final urban design proposals for the Avenue of Flags. The teams had prepared presentations involving the design objectives they focused on, proposed land uses and amenities, circulation, form-based codes, phasing strategies, and a 3-Dimensional animation of their design proposal, as well as 3-Dimensional renderings. The community was appreciative to all of the hard work that the students put into accomplishing their task of designing a downtown for Buellton on the Avenue of Flags.

This plan document contains the entire vision development process and urban design proposal of the team. It contains feedback heard from community meetings, the conceptual diagram, the proposed land use and regulation maps, the illustrative site plan, form-based codes, and three dimensional renderings. The document follows the process that the team went through in developing their urban design plan proposal for the Avenue of Flags.

chapter 2

site analysis





Figure 2.1: Location of Buellton in California

2.1 Project Site

Location of Buellton

Buellton is located on US Highway 101, in the Santa Ynez Valley, of Santa Barbara County (Figure 2.1). Within a six-mile radius of the town, are the four other towns of the valley: the City of Solvang, the western town of Santa Ynez, and the rural communities of Ballard and Los Olivos. The larger cities in the vicinity of Buellton include Santa Barbara, 40 miles to the southeast on Highway 101; Santa Maria, 35 miles to the north on 101; and Lompoc, Vandenberg Air Force Base and Vandenberg Village, 17 miles to the west along Highway 246. Los Angeles is two hours south of Buellton on Highway 101, and San Francisco is about a five-hour drive north on Highway 101.



Figure 2.2: The project site in Buellton, CA.

Location of Project Area within Buellton

The project area is located on the main strip of Avenue of Flags in Buellton. It starts at the exit of 101 onto the avenue and then ends where Avenue of the Flags meets US Highway 246 (Figure 2.2).



Figure 2.3: Pea Soup Andersen's located on the Avenue of Flags.

History

Buellton was founded on January of 1918 when a newly built bridge was opened across the Santa Ynez River. This bridge led to an intersection with the “Mission Highway” and the newly established route between Los Angeles and San Francisco, which became the intersection of Highway 101 and Highway 246 (“Mission Highway”). Buellton was the last of the five towns within the Santa Ynez Valley to be established.

The name “Buellton” was created from the name, Rufus Thompson (R.T.) Buell, upon whose land the town was established on. R.T. Buell came from Vermont and migrated to California in 1853 to mine for gold. When that didn't work out, he bought a quarter of the 26,000-acres of the Rancho San Carlos de Jonata. By 1865, R.T. Buell bought the entire 26,000-acre area.

Buellton began to rapidly change after the turn of the century, with more settlers coming to farm and start businesses. Anton and Juliette Andersen started one of these businesses, a restaurant called the Electrical Café. Juliette brought with her from France a recipe for pea soup, and this was the beginning of the now famous Pea Soup Andersen's Restaurant and Hotel (Figure 2.3). Now travelers have come to know Buellton as “The Home of the Original Pea Soup.”

On February 1, 1992 Buellton became an incorporated city. Buellton operates under a five-person city council as its form of government. The town has come a long way since R.T. Buell settled on the land; now the town of Buellton has a population of around 4,800 people, and is hoping for more growth (City of Buellton Website).

Brief History of the Project Area

The project area is the Avenue of Flags, which is the old Highway 101, before it was moved. The highway was opened through the heart of the town, lined with service stations, motels, and diners. In the 1930s, the strip of Highway 101 was improved as more traffic could be accommodated on the road. In the 1940s, the avenue was an eight-lane road through town, four lanes for local traffic, and four lanes for north and south travel on the highway (Figure 2.4). This newly widened highway and the location of it near all travel services, earned Buellton the name of “Service Town, U.S.A.”



Figure 2.4: Avenue of Flags, today.



Figure 2.5: Flags lining the center median of the avenue.

In 1964, Highway 101 was moved to the present day location, east of town, which left an abandoned strip through the center of town. In 1968, California Governor Ronald Reagan dedicated this strip of land to Buellton and named it “Avenue of Flags,” the name we know it as today (Figure 2.5).

This wide avenue has become a source of civic pride to the town of Buellton, with its bronze statues, state flags, American Flags, and flags representing and honoring each branch of the United States military (City of Buellton Website).



Figure 2.7: New mixed-use development located on the northeast side of the Avenue of Flags.

2.2 Existing Land Uses on the Site (Figure 2.6)

The project site is on the Avenue of Flags, which serves as the main street and downtown area in the City of Buellton, California. The avenue is accommodating to a number of uses, but also includes several vacant lots. To the north of the site is the on and off ramp to Highway 101 with gas stations and hotels in close proximity, as the avenue does provide services for trucks travelling through the city. To the west of the avenue are motels and a medium-density residential lot. This caters to the needs of the truck drivers that stay over-night as well as the low-income residents of Buellton. To the southwest of the avenue is Anderson's, which is the city's most advertised and well-known restaurant. To the east of the site are commercial developments including shops, restaurants and gas stations. To the northeast of the avenue is a new mixed-use development area (Figure 2.7), which presents opportunities for housing and new business owner needs. All of the buildings located on the site do not exceed two stories.

The land uses surrounding the site include low-density housing to the east and Highway 101 and Zaca Creek to the west. The avenue itself includes two-lane roads on either side of an approximately 75-foot wide median,

which serves as a park. There are no crosswalks or easy pedestrian access to the medians, as there is also limited sidewalk area to the west of the avenue. There are two parking lots to the east side of the avenue, but it is limited and not easily accessible. The Avenue of Flags' location and land uses present design opportunities and limitations for a new downtown area for the city of Buellton.

Opportunities:

The Avenue of Flags is located next to Highway 101, which provides direct access for visitors.

The Avenue of Flags is located next to Zaca Creek, which can provide the site with water features.

There are numerous vacant lots in the project site, which present commercial and mixed-use development opportunities (Figure 2.7).

The Avenue of Flags presents a large amount of open space for redevelopment and a new downtown area for the city of Buellton (Figure 2.8).

Constraints:

Trucks going through the avenue, creates noise and parking restrictions.

Limited amount of parking.

Limited pedestrian access to open space (Figure 2.9).



Figure 2.8: Vacant lots located on the avenue along with its location next to Highway 101 presents numerous opportunities.



Figure 2.9: Limited pedestrian access to medians.

existing land uses (figure 2.6)



2.3 Existing Circulation and Transportation Conditions on the Site (Figure 2.10)

It is in Buellton's background to be affected by transportation, the town is located next to a major freeway. The main "avenue" of the town was formally a highway. A majority of the project site is made up of vast stretches of asphalt. The majority of roads in the project site are clean and without cracks. The start of the project site begins at the intersection between Highway 246 and The Avenue of Flags. This intersection sees heavy and constant traffic flow. A sizeable amount of traffic then turns onto the Avenue of Flags. The Avenue of Flags is split into two, two-lane streets by considerably sized medians. These medians are roughly 500 feet long and 75 feet wide, and are made up of mostly grass, with one divider housing a local scenic walkway. Heading North, the Avenue of Flags widens to become a two-laned street. Parking is available on the right side, and consists of 90 degree angled spots. As one moves further along the Avenue of Flags, parking is parallel on the avenue, available now on both sides of the street. At this point, traffic is mild and consists of sedans, vans, trucks, and semitrailer trucks. At the end of the public park in the median lies an intersection on 2nd street. This intersection feeds the Avenue of Flags with decent traffic, as 2nd street leads into the residential heart



Figure 2.11: A north bound section of the Avenue of Flags.

of Buellton, as well as towards other businesses. Continuing along Avenue of Flags, one can see that parking is consistently parallel to the avenue, and is available on both sides. Minor breaks in the medians exist every 500 feet, allowing drivers the opportunity to



Figure 2.12: Store front parking toward the south end of the project site.

turn. The end of the site is marked by the U.S. 101 Off-ramp.

Along the entirety of the north bound lanes on the project site, pedestrian walkways and sidewalks are hardly existent. The traffic load heading south on the Avenue of Flags is much more than that of the north bound lanes seeing as the south bound lanes are fed by Highway 101. A majority of this traffic consists of large trucks and semitrailer trucks, seeing as Buellton houses one of the few diesel stops along the 101 in the southern area. A majority of the parking on the south bound lanes is parallel and exists on the medians. Pedestrian walkways are available on a majority of the southbound lanes, as there is an increased store front presence. A majority of the south bound lanes on the project site are doubled, with a third extra turning lane added around the 246 and Avenue of Flags intersection. Near this intersection, angled parking spots are present in front of store fronts, enclosed by a thin green median. Here, the pedestrian presence is abundant, and sidewalks follow accordingly. The only real pedestrian crosswalks exist on the Avenue of Flags and

Highway 246 intersection. Public transportation is limited, with no local services provided.

Opportunities:

Roads can accommodate for varying traffic loads.

Parking is abundant.

Viewsheds are free and clear.

Infrastructure is in good condition.

Constraints:

Pedestrian walkways are minimal.

Mobility is restricted because of large medians.

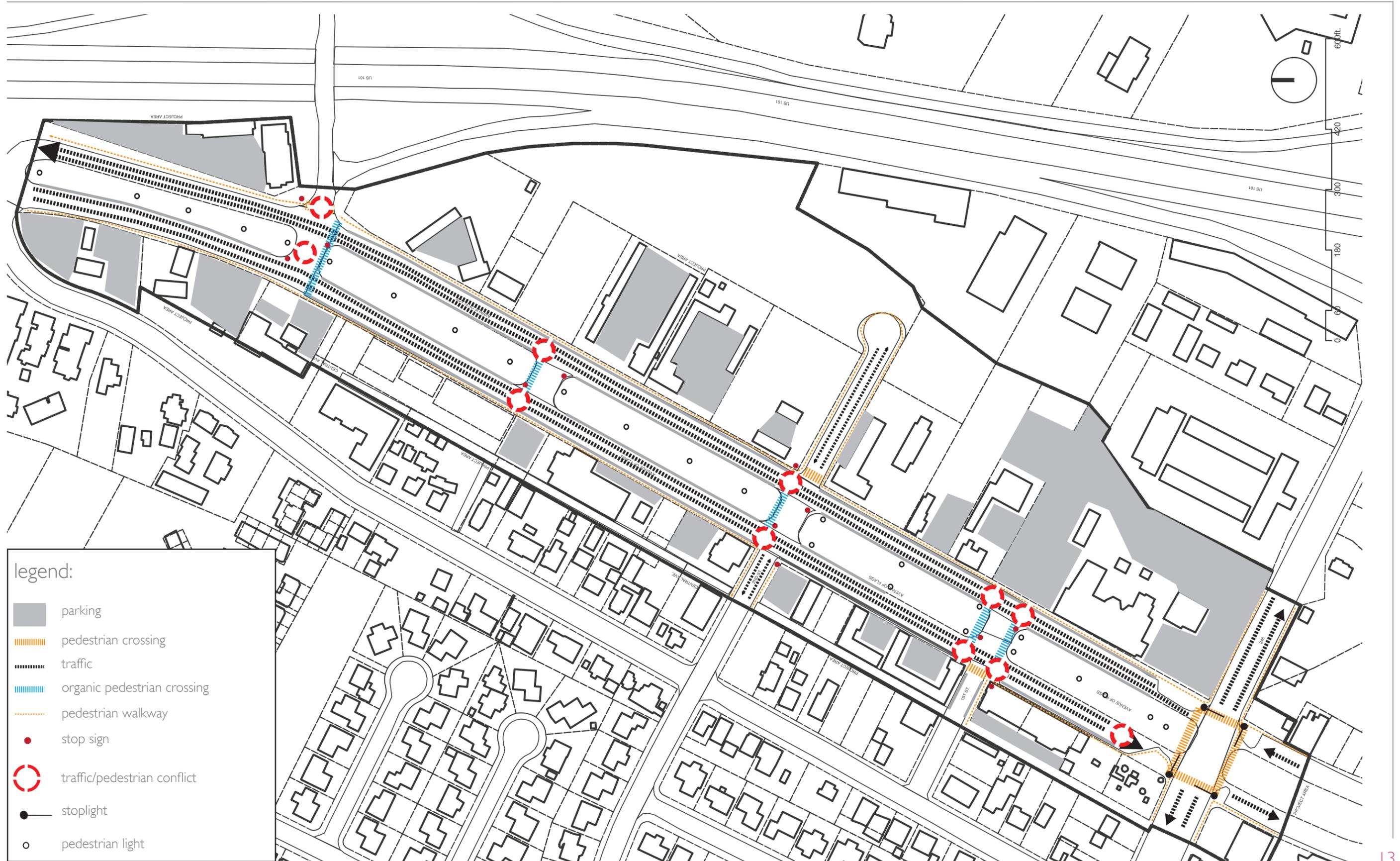
Public transportation is limited.

Pedestrian crosswalks are sparse.

Street parking exposes residents to fast traffic.

Semitrailer trucks are abundant.

transportation and circulation (figure 2.10)



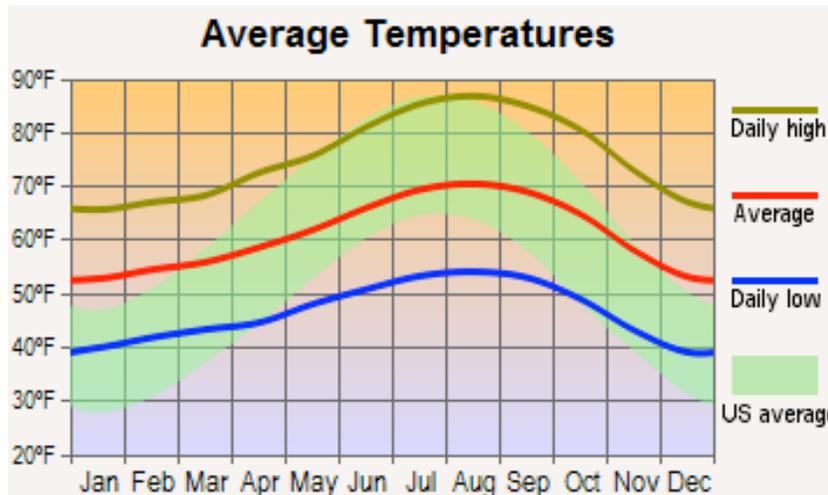


Figure 2.14: Average Temperatures in the Santa Ynez Valley retrieved from <http://www.city-data.com/city/Santa-Ynez-Valley-California.html>.

2.4 Natural Environment in and around the Site (Figure 2.13)

Local Climate

Buellton has a coastal climate with mild, dry summers and cool, wet winters. Typical summer temperatures are usually in the 80s and winter temperatures generally hover around the 60s. Winter lows are usually in the 30s with occasionally dropping below freezing (Figure 2.14). Between the months of November and March each year, Buellton usually receives an average of about 13 inches of rainfall (Figure 2.15).

Native and Existing Vegetation

There is almost no irrigated farmland in the Buellton area; few if any of the surrounding agriculture operations employ aerial spraying of fertilizers or pesticides, lowering air quality. The basic plant communities are native to the Buellton area, oak woodland and riparian wetlands. Most of the vegetation communities within the city have been introduced and maintained by humans, i.e. lawns cultivated raw crops, and other agricultural fields (Buellton General Plan).

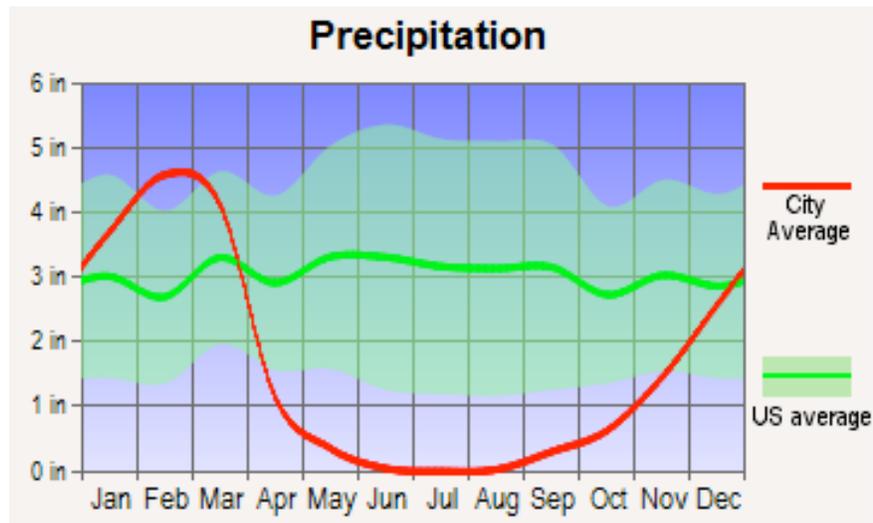


Figure 2.15: Precipitation in the Santa Ynez Valley retrieved from <http://www.city-data.com/city/Santa-Ynez-Valley-California.html>.

Visual Resources

Three major components of the Buellton visual resources are the topography, the vegetation, and the character of its built environment. The parallel ridgelines to the north and south provide a scenic backdrop to the community, while the surrounding fields establish a pastoral feeling. The urban character is much less defined. Commercial buildings have no particular style or theme, but rather reflect the individual tastes of owners at the time of construction. Vacant lots and gas stations create gaps in the commercial frontage along Avenue of the Flags, but the very wide median strip is a strong and unifying element to the avenue. Surrounding hills provide a very important visual backdrop to Buellton.

Sun Trajectory

Sun paths come from the east at about eighty degrees, and set in the west around two hundred and eighty degrees both to the True North.

Noise

Principle sources of noise in Buellton are its major highways, the US 101 freeway and Highway 246. Because of slow speeds and low vehicle counts, the noise levels on the other streets in the town are not considered big contributors to noise. Buellton has some industrial businesses that could produce noise

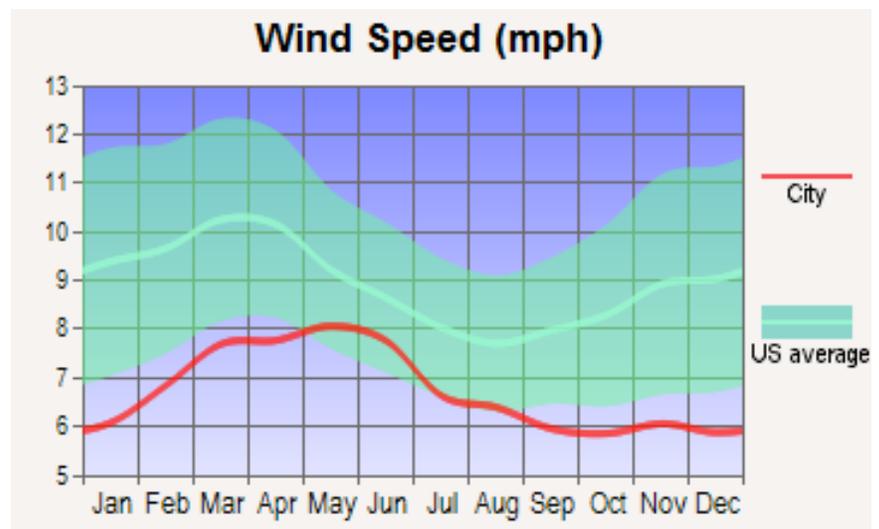


Figure 2.16: Wind Speed in the Santa Ynez Valley retrieved from <http://www.city-data.com/city/Santa-Ynez-Valley-California.html>.

considering where they were locating, such as near sensitive land uses.

Prevailing Winds

Northeasterly sea breezes in the afternoon blow down the Santa Ynez Valley (Figure 2.16). This cycle of sea breezes is particularly evident in the summer months when winds are stronger and last longer. Santa Ana winds note a condition in which the prevailing winds switch from northwesterly to northeasterly, bringing unusually hot, dry weather to the central coast. Point Conception marks a dramatic shift to an east/west shoreline. This makes the prevailing winds less to more highly variable winds along the coastal strip (Buellton General Plan).

Water Flow

Major surface water feature is the Santa Ynez River. Zaca Creek is another predominant water feature. An additional water source is the ground basin within the city limits. Buellton Uplands Groundwater Basin is a very deep groundwater basin, lying about 1000 feet deep.

Opportunities

Buellton's location next to the major highways can bring in business.

The avenue's slope can help terrace buildings in the redevelopment process.

Beautiful corridors into and on looking from/to site.

Giant groundwater basin from Zaca Creek and Santa Ynez River.

Climate is very pleasant.

Constraints

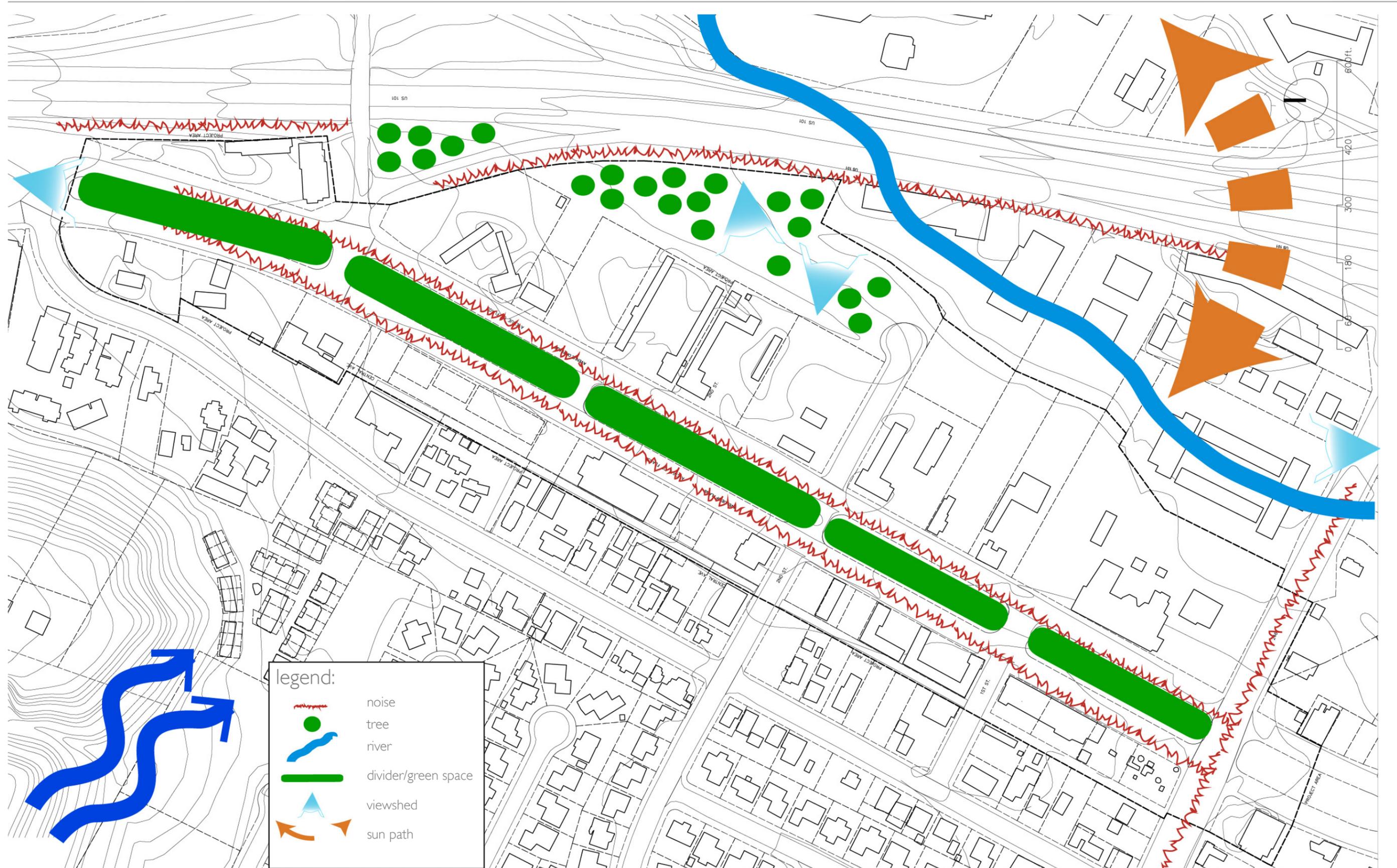
Noise pollution from highways.

Intense winds.

Most vegetation has been introduced, not native.

Town does not have an attraction for people to want to stay and visit the area.

natural elements (figure 2.13)



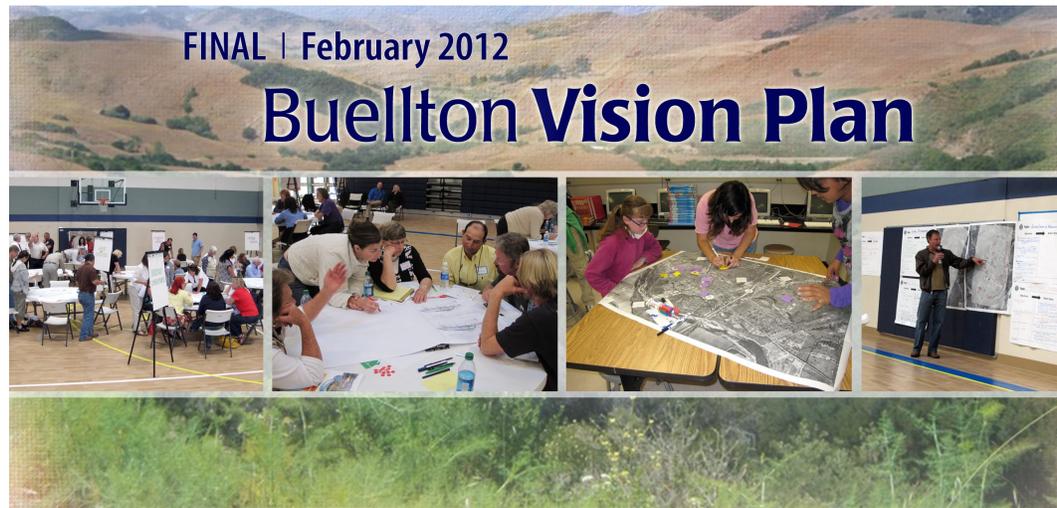


Figure 2.17: The cover of Buellton's 2011 Vision Plan for the Avenue of Flags.

2.5 Relevant Documents

Over the past few years, the City of Buellton has been working on the development of a Vision for the project area. There have been two vision plans created, following numerous workshops and collaborations with the community to finalize what residents wish to see on the site. The first visioning process took place in the early 2000s, however the vision plan was never implemented. A new vision plan was released for the project area in 2011 (Figure 2.17).

In 1993, Buellton formed a redevelopment agency, with the sole purpose of developing with a plan to improve the Avenue of the Flags/ Highway 246 area of the city. The rationale for redevelopment was to revitalize the community as well as the economy, by creating a central area to draw in residents and visitors to the town. In 2001, the city planning commission met with local community members to discuss the topic of redevelopment in the city, after many meetings they developed key objectives they wished to accomplish.

One of the most important goals of the visioning process was to create a “gateway” to the area that made the Avenue into a friendly downtown. There are not many developments drawing in customers to the area, so planners saw it as very important to create a downtown with an inviting atmosphere, as an entrance

into the city. Also, there is a large grass median running along the entire Avenue that does not get much use, so the redevelopment agency planned to develop it in a way that would increase the median use and make it more aesthetically pleasing. In addition to developing the median, the agency planned on redesigning the overall structure of the Avenue, by widening sidewalks, reducing lanes, and enhancing pedestrian access.

It was important to the city, as well as the community, that the development follow all guidelines set out in the first General Plan of 1993. This included an overarching theme to the development that reflective the quaint feeling of the town. It also set regulations for appropriate setbacks for all structures to be built along the Avenue. The General Plan also laid out general rules for the layouts and placement of trees, pedestrian crossings, and design of parking areas. This design plan was never implemented along the Avenue.

In 2011, the city Planning Commission met, and a new zoning ordinance focusing on mixed-use developments was enacted. The

which offer many recreation opportunities for people, and the planners hope to capitalize on that aspect of the city.

After various community meetings and workshops, the vision statement was developed, which outlines the wishes for redevelopment, as well as individual goals to achieve the vision. It is very important to the city that they create a unique area, to give Buellton its' own identity, apart from other cities near by. Currently, many people see Buellton as a town they drive past while cruising down Highway 101, and the city hopes that after redevelopment, it will become a destination that vacationers will want to stop at. The city hopes to achieve this by designing a downtown with an exciting atmosphere. The city wishes to create an attractive downtown that offers daytime activities, as well as nightlife for those who wish to go out and have some fun in town. In doing so, they hope this downtown will continue the sense of community that is of the utmost importance to the city of Buellton.

Buellton focuses greatly on active lifestyles, and they wish to expand the idea of healthy living throughout the city. To do so the city plans on providing increased alternative means of transportation besides vehicles, by creating a more pedestrian and bicycle friendly area. The vision plan also calls for new recreational facilities to be built. In addition to the importance of healthy living, Buellton is

very proud of its art culture. The city hopes to develop the Avenue in a way to support local artists, hold special events, and showcase artwork.

With the growing worry in the nation about the environment, Buellton hopes to take the lead in being an environmentally conscious community. The city has great views of the surrounding hills that it hopes to keep pristine for the future. The vision hopes to implement green technology to not only be environmentally sustainable, but economically sustainable as well. Lastly, the vision plan wishes to support local businesses to foster the local economy and residents, supporting the Buellton community.

chapter 3

conceptual development



Introduction

The conceptual development process started with an ideation, which was followed by conceptual diagramming and a community meeting. Following the meeting, findings were analyzed and the diagram was revised accordingly.

To create the ideation, there was a need to bring together “images of life” that would be appropriate snapshots for future life on the Avenue of Flags. Based on these images, the suitability of each image was collected and discussed on its fit for the project area. These images serve as the design concepts for the site that support the elements of the conceptual diagram.

From these urban design concepts the development of the conceptual diagram was able to begin. The diagram was based on the site analysis from the previous chapter and the design objectives. The objectives covered the land uses on the site, the circulation, public space structure, urban design, sustainability and natural resources, and finally the implementation and phasing of the entire site. The conceptual diagram included the proposed land uses, their location and interrelationships, the circulation of the vehicles and pedestrians, proposed public spaces, landmarks, and the connections of the site area to its surroundings. The conceptual diagram that was put together from all these

aspects were then presented to the residents of Buellton at the second community meeting.

The feedback given at the community meeting was analyzed, and the conceptual diagram was revisited and adjusted to what the residents displayed would be best suited for their town.

3.2 Ideation

The team began the conceptual development process by visiting the city of Buellton to meet with community residents. The purpose of this was for the team to be able to talk with the community members to gain an understanding of the community's needs and wishes.

The meeting began with residents addressing their current vision for the site, as well as addressing existing issues they wished to see improved. To get a better sense of what the community liked about the Avenue, as well as what they wished to see improved, the teams created "have" and "wish" poems. For each of the poems community members wrote down what they liked as well as what they wished they Avenue had. After further commentary and reviewing the poems, there were a

number of issues that clearly needed to be addressed in the conceptual development process.

In addition to the discussion, the team handed the community members each two cameras, one for features the Avenue that they liked, and the other for features they wished to see improved. While the community residents walked around taking pictures, the team mingled with them, discussing the site, to get a deeper view of what needed to be addressed (Figure 3.1). Interacting with the community members was crucial to the development process because the team was able to connect with the residents to understand what was important to them on the Avenue.



Figure 3.1: Team members Kimber Gutierrez and Thomas Vogt interacting with the community members.

After the meeting was adjourned, the team reconvened to discuss what was discovered by visiting the site and meeting with the community. Some of the most important aspects that the community brought up include being pedestrian friendly, creating a plaza, slowing traffic, having a water feature, and creating an overall unique downtown feeling (Figure 3.2). The team took this into consideration and worked to develop with a conceptual diagram and urban design concepts that reflected these aspects.

Following the first visit to Buellton, the team developed a conceptual diagram centered around a town square, which included public amenities such as a library, museum, civic center, as well as shops and restaurants (Figure 3.3). The



Figure 3.2: Pedestrian friendly traffic circle to enhance walkability and to slow down traffic congestion.



Figure 3.4: Proposed paved sidewalk, lined with retail to replace the center median.

most significant change that was proposed in the diagram was the removal of the center median, and converting the area into a paved sidewalk, lined with commercial businesses on either side for visitors to peruse (Figure 3.4). The rationale behind this was to create a pedestrian friendly zone. Restricting the street to only the west side, with one lane in each direction, would drastically slow down traffic and allow pedestrians to walk around much more safely.



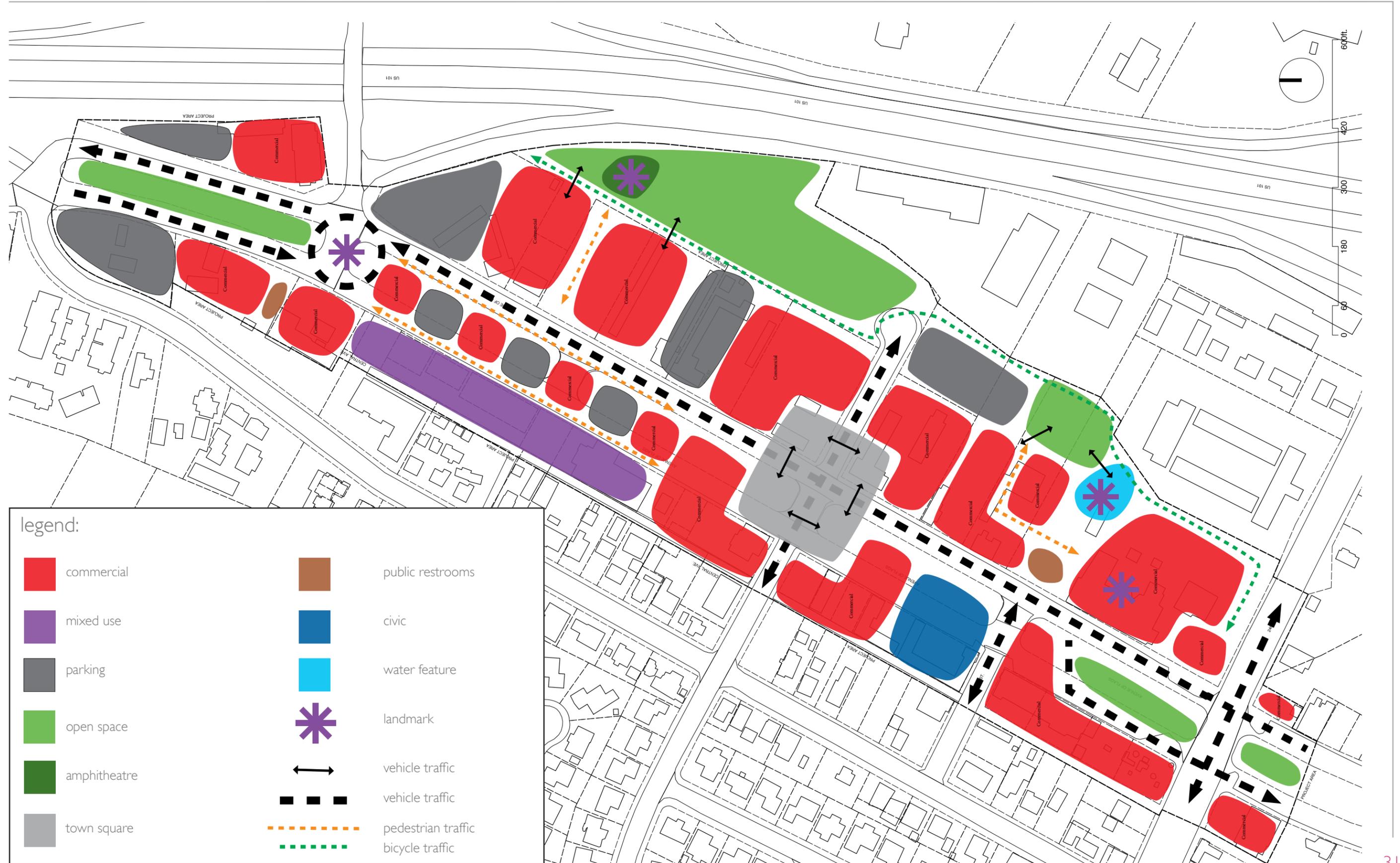
Figure 3.5: Multi-use green space for the community to hold various events.

In addition to the town square, the Avenue was divided into four districts, each with a different function for the different age groups. The districts include a travel friendly center, an area for nightlife, a commercial district, and a family oriented district. Towards the entrance to the Avenue the shopping district was proposed, containing various commercial businesses. The community expressed that they would like to create nightlife for Buellton for residents who wished to stay out later. To accomplish this, the team proposed a section of the site that is to contain more upscale restaurants and bistros that would be open for lunch during the day, but would convert to restaurants that are open late in the evening. Currently, the area of the site closest to the freeway remains an empty lot that used to be a zoo. The team proposed an area of open green space for public use, hold various events. (Figure 3.4). The southern most district

located next to Pea Soup Andersen's was proposed to be for family usage. The team proposed family oriented restaurants and shops, such as candy stores and ice cream parlors. Lastly, the team aimed at creating a way to calm traffic coming off of the freeway, as well as to accommodate travelers.

The team achieved this by proposing a gas station, commercial shops, and a hotel, directly off the Highway 101 so people traveling would have easy access to these amenities. In addition, the team proposed a roundabout to calm the traffic from fast moving cars exiting the freeway. The rationale behind this was to slow down cars as they entered the Avenue to keep pedestrians safe and accommodate for the proposed lane reduction.

conceptual diagram (figure 3.3)



3.3 Community Meeting II Findings

During the site visit on March 28, 2012 community members were asked to identify their wishes about the future of the Avenue as well as the features that they are satisfied with. Prior to the second community meeting these findings were summarized by the team on 24x36 inch posters.

On Monday, April 16, 2012, the team met with Buellton community members at the city's recreation center from 2:30 to 4:00 pm, to present their conceptual diagrams (Figure 3.6). After setting up their conceptual diagram poster, the team interacted with the members of the Buellton community at the city's recreation



Figure 3.6: Community Meeting II held at Buellton's recreation center on Monday April 16, 2012.

center. To start, the community members and teams were gathered together for a brief introduction of the purpose and intended activities for the meeting. Next the community members reviewed the different posters to interact and give feedback about the teams conceptual diagrams and initial design ideas (Figure 3.7). The meeting continued with two activities.

The first activity allowed the community to pick their favorite existing and desired elements from the 24x36 inch posters for the Avenue of Flags discussed in the first community



Figure 3.7: Team members Shelby Messner and Lauren Shafer interacting with Buellton community members.

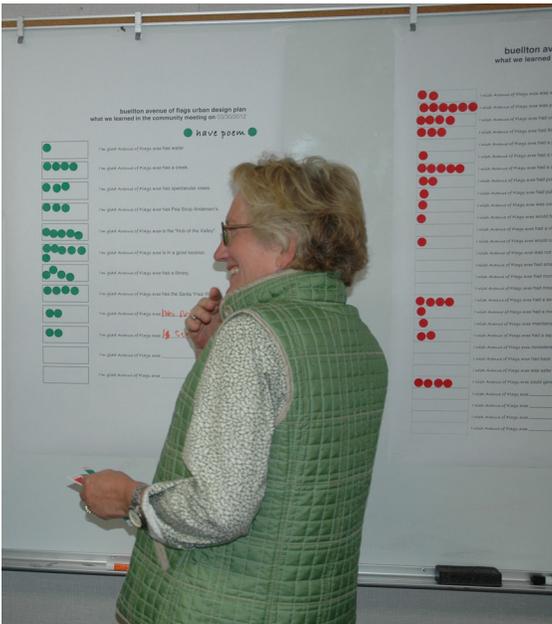


Figure 3.8: Community member participating in the colored dot activity.

meeting. They did this by placing colored dots by the statement they believe to be most important to consider in the development of the avenue (Figure 3.8). This allowed the teams to see what the community wants and what they think would be of the most important significance to the design of the avenue. Green dots were used for the “have poem” and red dots were used for the “wish poem.”

The top three choices for the “have poem” include:

I’m glad Avenue of Flags area has a creek.

I’m glad Avenue of Flags area is in a good location.

I’m glad Avenue of Flags area has the Santa Ynez River.

The top four choices for the “wish poem” include:

I wish Avenue of Flags area was pedestrian friendly.

I wish Avenue of Flags area had unique (architectural elements) and restaurants/ retail.

I wish Avenue of Flags area had the 4 “P’s”: Potty, Parking, Plaza, Party.

I wish Avenue of Flags area has a creek connection and natural hiking.

This allowed the teams to see what the community wants and what they think would be of the most important significance to the design of the avenue.



Figure 3.9: Community members working with the team to create their “ideal Avenue of Flags.”

The second activity had the community members develop their ideal avenue conceptual diagram using different colored markers to identify different land uses (Figure 3.9). This activity allowed for the teams to visually see what the community wants on the avenue.

After the community members finished their conceptual diagrams, they were asked to present them to the entire group of participants (Figure 3.10). This allowed for the whole community to hear different options and views on what should be done to the avenue. The purpose was for the teams to get feedback from the community members to improve their conceptual diagrams.

The team received positive feedback from the community members and made changes to their conceptual diagram accordingly. Having originally proposed a movie theater and bowling alley to enhance the downtown area’s attraction, the community members expressed concern about such venues because of existing theaters in the area and the typical building façade these buildings would have. Instead, the community



Figure 3.10: Community member presenting urban design ideas to the rest of the Buellton community members.

members suggested street vendors, sidewalk musical entertainment, or a smaller scale theater to host plays or musicals to liven up the downtown area. Another Buellton resident made the suggestion to have parking districts in between retail stores and offices to provide closer parking to not only the stores, but also the pedestrian friendly alleyways. The concern that was heard the most by the team was the original design idea of blocking off 2nd street to the avenue to provide more of a pedestrian accessible downtown. During the community meeting it was brought to the team’s attention that 2nd street is used very often and to close it would increase traffic congestion and limit the accessibility to Buellton’s recreation center, which is located on 2nd street. These concerns were debated and discussed by the team and members of the community and were addressed in the process of redeveloping the conceptual diagram.

chapter 4

land use, circulation, and
public space structure



Introduction

Chapter 4 introduces the decisions of land use, circulation and public space structures by the team. These decisions were developed into an illustrative plan study and a proposed land use map. Section 4.1 presents the community meeting and its findings. Section 4.2 introduces the locations of the proposed land uses and regulation areas by the team. Section 4.3 proposes the various circulation patterns and domains. Lastly, section 4.4 covers the numerous community amenities proposed by the team for the Avenue of Flags.



Figure 4.1: Team members Kimber Gutierrez and Thomas Vogt presenting the team's illustrative plan study to Buellton community members.



Figure 4.2: Team members Lauren Shafer and Shelby Messner presenting the team's proposed land use map to Buellton community members.

4.2 Community Meeting IV Findings

On Monday, April 30th, 2012, the team met with Buellton community members at the city's recreation center from 2:30 to 4:00 pm, to present their most recent design development. Approximately 35 community residents came to the meeting to review the developments. To begin, the community members were given an introduction of the purpose of the meeting and were instructed to spend time examining and providing feedback on each of the team's designs. The community members rotated around the room, spending approximately fifteen minutes with each team, to discuss the illustrative plan studies and proposed land use maps that they were presenting.

The community members analyzed the team's illustrative site plan as well as their proposed land use map (Figure 4.1-4.2). The illustrative site plan individually laid out where the team would like to see development in the future for specific buildings and activities. This allowed for community members to have a visual



Figure 4.3: Community members giving feedback to the team's illustrative plan study.

representation of the team's vision for the future of the Avenue.

Both the illustrative site plan and the proposed land use map were intended to allow the community residents to be involved in the development process by giving the residents the opportunity to provide valuable feedback to the team. After meeting with all of the community members that participated in the meeting, the team received very positive responses on their design development. The residents were very fond of the traffic circle as a traffic-calming strategy, and the fact that the team reduced the Avenue down to only two lanes. The community saw this as very positive because they wish to have a pedestrian oriented downtown, which was the team's vision.

In addition, most residents were in favor of the bike path running along the open

space in the team's design, they even suggested connecting it with the existing path to unify it. In the open park the team proposed a water feature for children to play, and this was a favorite of many of the residents. There were even two children who visited the meeting, and were very intrigued by the idea of a water feature. Many residents mentioned ensuring that there was a suitable amount of parking to fit the requirements of the proposed commercial developments, and recommended possibly adding more parking options. Another resident brought up the issue of high wind patterns that Buellton faces, and suggested building infrastructure in a way that would block the wind from patio seating.

The community meeting was very positive in helping the team continue their design development.

4.3 Proposed Land Uses

The team created a proposed land use map that depicted what building uses they proposed for each area of the Avenue. The proposed land uses map divided the site area into color coded sections to display what development the team intended for the section. The illustrative plan study gave the community members a more specific view of building type that the team has in their vision for the site.

The main focal point of the site area in the team's design is a town square, located directly on the Avenue, intersecting with Second St. Surrounding the town square, the team proposed various commercial shops as well as a hotel overlooking it, to reinforce the "square" shape and feel. At the entrance to the Avenue, coming off of Highway 101, the team proposed an area for travelers. This district includes, a diner, gas station, and a hotel. The team proposed a roundabout to be located below this to slow down traffic driving onto the Avenue, which they proposed be made smaller with only one lane in either direction. On the Western side of the Avenue the team designed a commercial area composed of mixed-use developments as well as shops, connected by a pedestrian alley. On the East side of the street the team proposed a commercial center with restaurants, shops, and an art gallery. This district was intended to contain establishments that stay open later at

night, so residents who wish to go out have a place to congregate. The team proposed that these establishments would have outdoor patio seating, overlooking the public open space they wish to see developed. This open space would be used for community events and gatherings, and would connect to another green space the team proposed among the lower end of the site connecting to Andersen's. Parallel to Andersen's, the team has proposed a commercial center that is intended to be family-oriented. This would include ice cream parlors, candy stores, and family-friendly restaurants. On the South/West side of the Avenue the team proposed a civic center, intended for public use. Beyond the civic center, the team proposed more commercial retail buildings. The team also proposed many commercial establishments in order to stimulate the economy of Buellton, and create a friendly downtown atmosphere.

4.4 Circulation

A key component to the redevelopment of the Avenue of Flags rests upon an increase in pedestrian activity. The developed Illustrative Plan Study highlights this, illuminating multiple pedestrian paths, increased pedestrian-street awareness, and an overall more people friendly feel. One of the most notable changes is also one of the most vital to the project site. This change consists of a town square with elevated and “marked” pavement. The town square is elevated so that vehicles, upon entering, will be alerted of a change of priority, one which belongs to pedestrians. The pavement in the town square will also be textured or marked, so that visual and auditory cues will be present whenever a vehicle does cross the town square. Not only does this create a more pedestrian friendly environment, but it also encourages safe driving habits and driver awareness.

Another proposal to the Avenue of Flags is the removal and reshaping of a present street into a pedestrian walkway. The current two lane street that runs North to South would be readapted into a pedestrian walkway, with the traffic from that street being redirected onto the South-North corridor. The adaptation of the two lane street into pedestrian space is also cushioned by an increased presence of commercial space, in the hopes of inventing a large street presence for the planned

commercial parcels. This pedestrian walkway will be the most heavily used, as it connects the beginning of the project site to the town square, effectively serving as a bridge between the two anchor points.

Another pedestrian walkway has been created in the lower southwestern portion of the site which runs almost all the way through the Avenue of Flags. This walkway is not as important to a pedestrian stand point as the other mentioned walkways due to the fact that this southern walkway houses almost all bike traffic for the site. The beginning of this walkway features a bike rental shop for visitors, and ends at the Buellton War Memorial. This walkway also runs through a park and water feature that exist on the most southern quadrant of the project site. At the water feature, the bike/pedestrian path connects with another pedestrian path, one that runs straight through a “small town” styled alleyway of shops and boutiques. Before ending at the Memorial, the bike/pedestrian path also parallels an elevated patio reserved for businesses that require outdoor seating or a more intensive street presence.

While pedestrian needs are extremely important to the projected site plan, vehicular needs also demand a strong presence on the Avenue of Flags. The team proposes that the North-to-South lanes be converted into pedestrian walkways, with the traffic from those

lanes being redirected towards the current South-to-North lanes. The South-to-North lanes will be split into two, one-way lanes, at 12 feet apiece. The lanes join together to create one street so that commercial presence can be further exploited and presented. Pedestrian walkways will be present throughout the traffic lanes, and will be denoted by raised and textured pavement. The town square proposed inside the site on the intersection of 2nd Street and the Avenue of Flags, and would possess traffic stoplights that activate on all streets when pedestrians would like to cross. Later, as the street reaches the most southern section of the site, the lanes split and assume the original positions presented on the Avenue of Flags, with the exception of only one lane existing in each direction.

Parking that exists on the project site is proposed to be “hidden” in lots that exist behind their respective businesses. By removing the parking from street-sight, a more personal and commercial oriented would be created. A roundabout is proposed on the northern half of the site that could accommodate for semitrailer tucks, as well as smaller vehicles. An alley/ street is proposed behind the mixed use developments that eventually connects with 2ⁿ street. This alleyway would serve as a feeder for the parking available behind the proposed developments.

In conclusion, the Avenue of Flags project site is proposed to be heavily geared towards meeting pedestrian needs and wants.



Figure 4.3: Proposed art gallery.

4.5 Community Amenities

Green Paved Walkways

The Multi-Use Open space, located on the eastern side of the site, is proposed to have green paved walkways, which are short trimmed grass areas with stone walkways over them. This will create a friendly surface for farmers markets, car show, and other festivals Buellton might have.

Art Galleries

Within the commercial (nightlife) district, art galleries are proposed to host local Buellton artists, and other artists from the region.

Outdoor Amphitheater

On the northern edge of the Multi-Use Open space area an outdoor amphitheater is proposed for community concerts, shows, and other events that Buellton might host.

Brick Crosswalks/Town Square

A town square is proposed where the Avenue of Flags intersects 2nd Street (Figure 4.4).

Bike/Walking Path

On the eastern edge of the site a bike and walking path along the entire edge is proposed, reaching from the south of the site all the way to the Highway 101 on ramp. The bike path is intended to connect with the existing bike path on the other side of the highway in Buellton;



Figure 4.4: Raised stone crosswalk the team is proposing for the town square

this would promote biking and other outdoor activities in the city.



Figure 4.5: This proposed water feature will be located on the site's multi-use green space.

Water Feature

A water feature is proposed for the public park area on the eastern side of the site. This is intended to be a children friendly water feature, where the water will shoot up from spouts in the ground, enabling the children to run through them on a hot day (Figure 4.5).

Civic Center

The civic center is proposed on the western side of the town square within the Avenue of Flags. It is proposed to contain a public library, historic society or museum, and community center.



Figure 4.6: Dog park along with the proposed green pavement for the multi-use gree space.

Public Park Area

Behind the proposed family commercial district there is a public park proposed. In the park, picnic areas, a playground, a children's water feature, dog park, and public restrooms are envisioned for this area.

Dog Park

Located within the public park a dog park is proposed. This feature would have a play area for the dogs and doggy bag stations for visitors to use (Figure 4.6).

chapter 5

form-based codes and
illustrative site plan



Introduction:

The objective of Chapter 5 is to develop form-based codes that will help achieve the desired public space along Avenue of Flags, as well as create a phasing strategy for future implementation of the proposal (Figure 5.1). This chapter discusses the rationale behind each of the form-based codes, the 3-fold phasing strategy (Figure 5.2), low impact development proposals, as well as circulation throughout intersections. In addition, Chapter 5 contains a series of 3-Dimensional still renderings created by the team to illustrate the urban design proposal. (Figure 5.3)

regulating plan (figure 5.1)



Phasing Strategy

Phase One: 0-5 years

Lay down textured paving for the town square and crosswalks along the avenue

Renovation of existing open space – clean up, demolish old structures

Start construction on the northeast side of Highway 246 and continue north to the southeast section of the proposed town square

Construction on the south side of Highway 246

Phase Two: 5-15 years

Narrow the first median at the north end of the site along with constructing the traffic circle

Construct a new road that connects the Avenue of Flags with Central Avenue and that leads to the alleyway behind the mixed use and commercial buildings

Continue renovation of open space with the implementation of the bike path along with the extension of the open space to Highway 246 - start laying the permeable pavers for circulation areas in open space

Close the lanes on the west side of the median and create the main two-lane road through the avenue

Start selling the properties acquired by closing the west side of the avenue and the medians

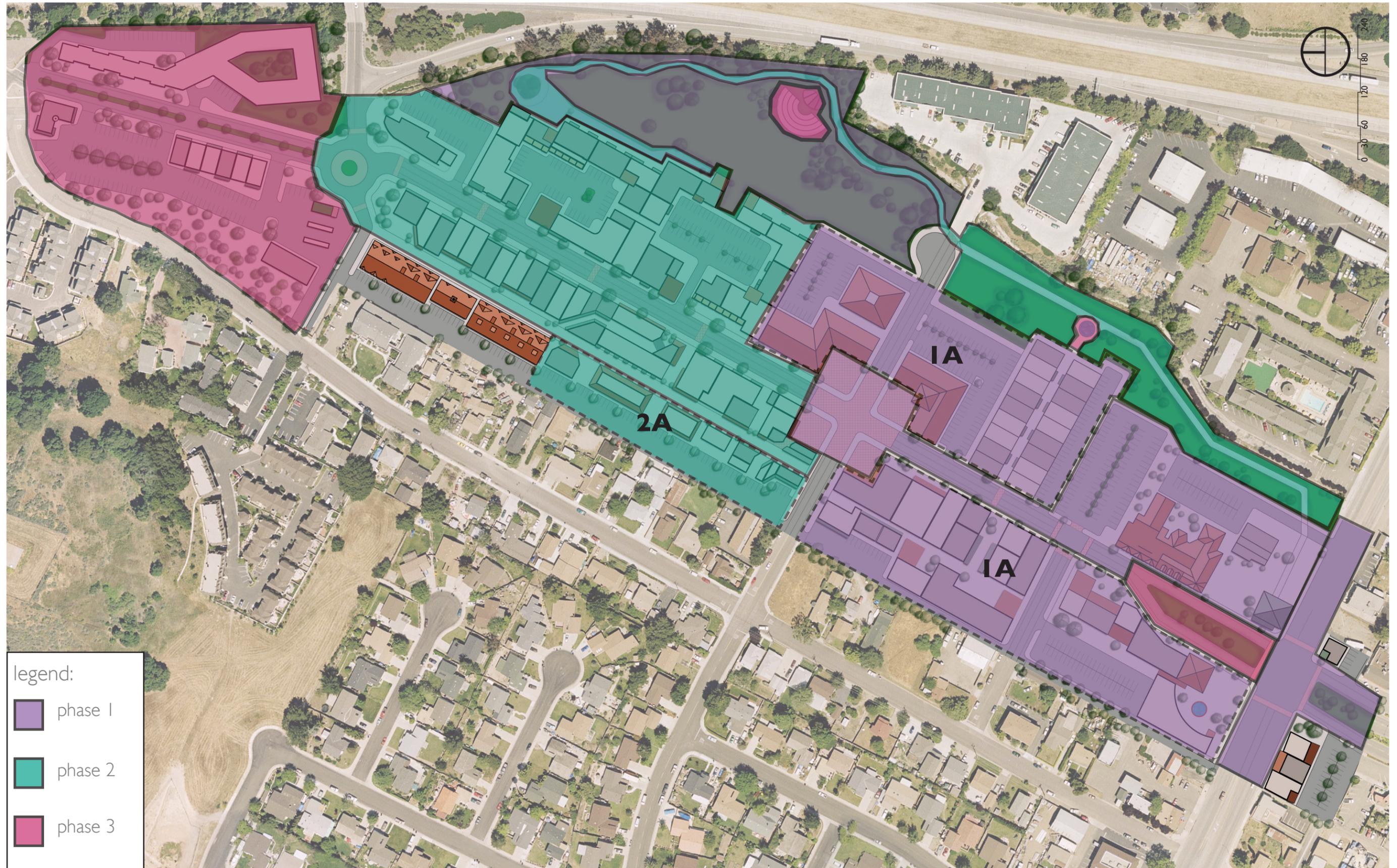
Phase Three: 15-30 years

Finish constructing the traffic circle and shape the median on the south end of the site to match the avenue on the north side of Highway 246 with the south side of Highway 246

Continue renovation of open space by building the outdoor theater and children's water feature

Construct the travelling district on the north end of the site

phasing plan (figure 5.2)





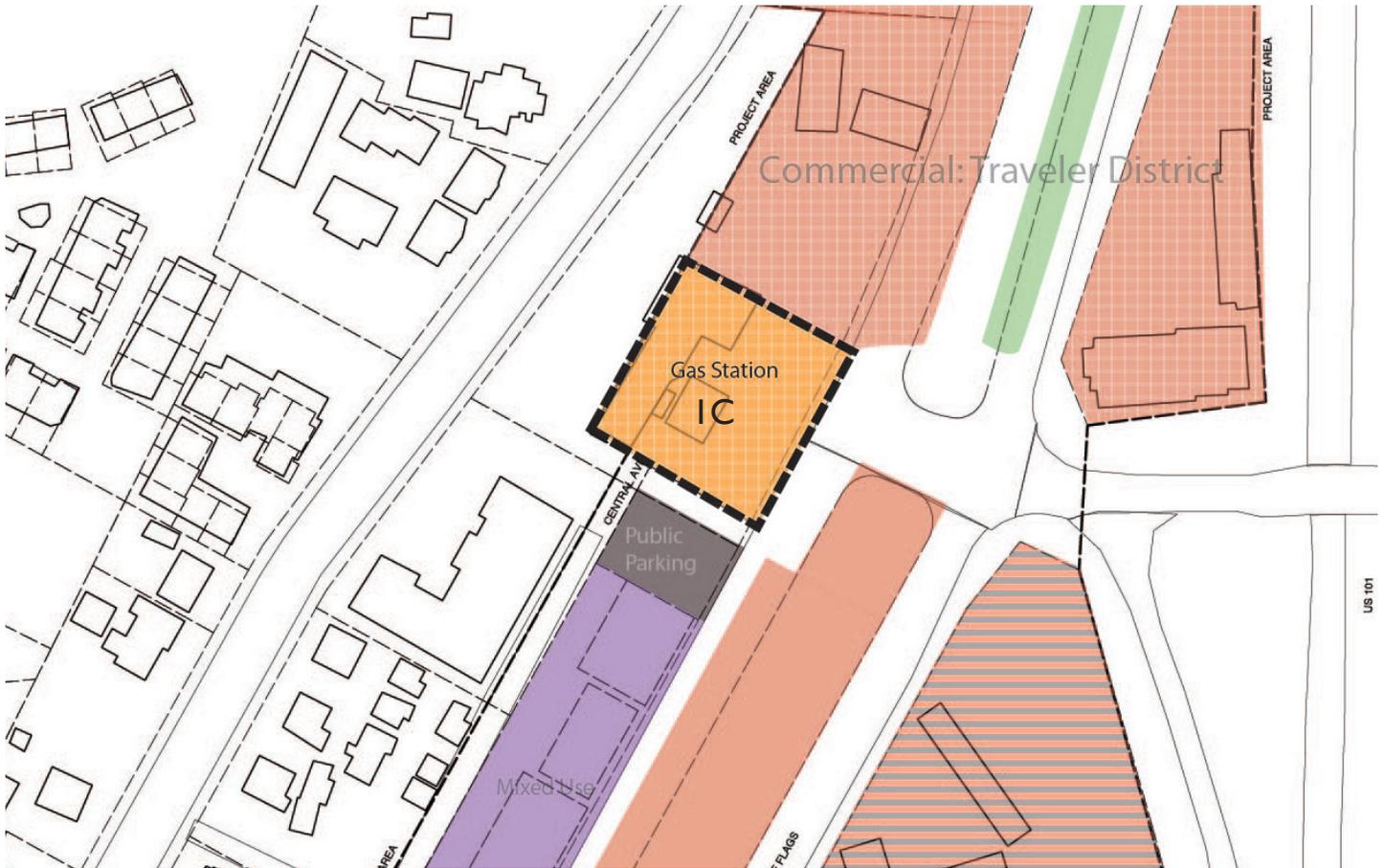
Name of Regulation Area: COMMERCIAL: TRAVELER DISTRICT

Urban Design Objectives for the Regulation Area:

The commercial defined in this part of the FBC is intended to support the activities that would take place in the proposed Traveler District area. Throughout the community meetings in Buellton, the residents have emphasized the importance in keeping trucks off of the Avenue, while still attracting and inviting people to stay and enjoy the project area.

The urban design team’s objective in proposing this traveler oriented commercial area is twofold:

- 1. To provide for travelers needs, but attracting visitors to linger with unique retail development.*
- 2. To help decrease truck parking and over-night stays on the Avenue by providing adequate parking away from the main road, as well as a hotel.*



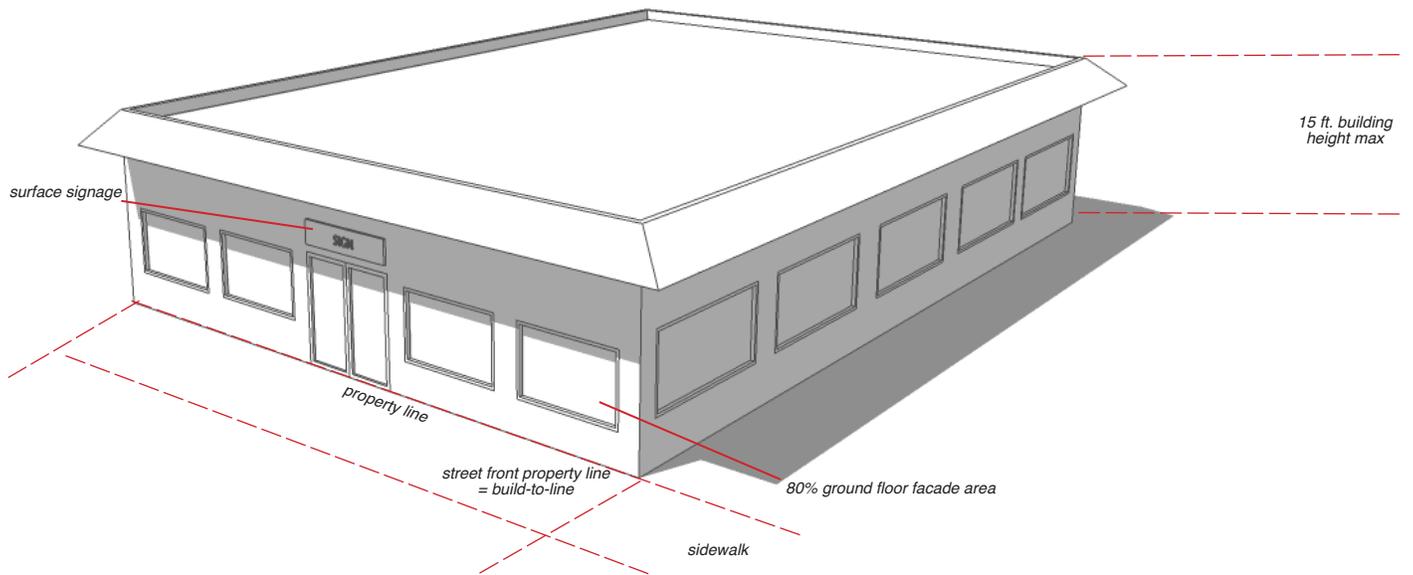
Name of Regulation Area: COMMERCIAL 1C: TRAVELER DISTRICT (Gas Station)

Urban Design Objectives for the Regulation Area:

The gas station defined in this part of the FBC is intended to provide of a service station at the north end of the Avenue in the proposed Traveler District area. Throughout the community meetings in Buellton, the residents have emphasized the importance of keeping trucks off of the Avenue, and the team wants to provide services for the trucks close to the highway. Building siting and massing information will be determined by service station regulations.

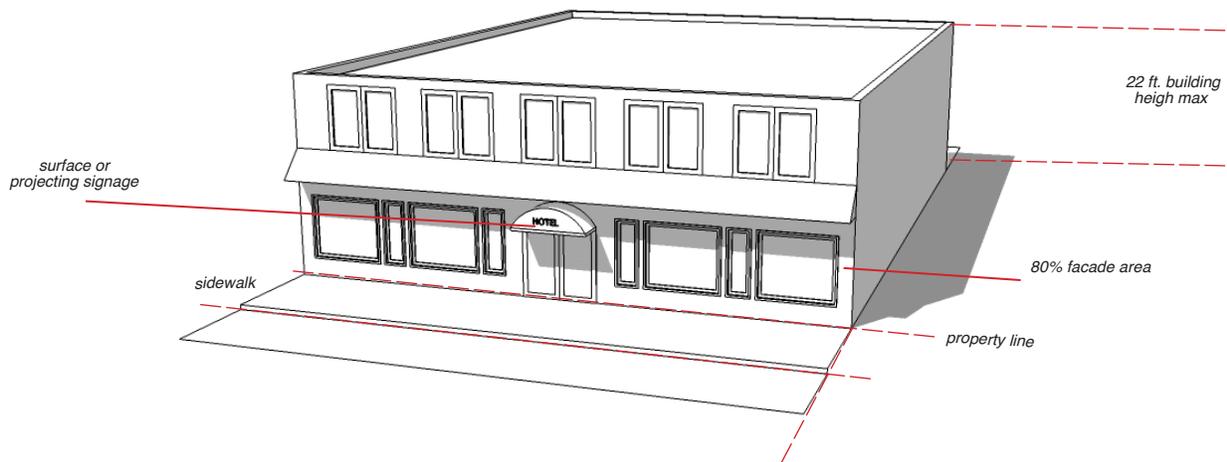
The urban design team's objective in proposing this gas station is to:

- 1. To provide for travelers needs.*
- 2. To help decrease truck parking and over-night stays on the Avenue by providing a service station at the north end of the site.*



COMMERCIAL: TRAVELER DISTRICT 1A | Building Siting and Massing

ALLOWED LAND USES	
Ground floor	Commercial
SETBACKS	
From street front on ground floor	0 ft. maximum
From side property lines on ground floor	30 ft. maximum North 0 ft. maximum South
From back property line on ground floor	80 ft. maximum
BUILD-TO-LINES	
On street front on ground floor	Street front property line = build-to-line
On side property lines on ground floor	South side property lines = build-to-line North side property lines = none
On back property line on ground floor	None
BUILDING HEIGHTS	
Ground floor (minimum and maximum)	12 ft. minimum, 12 ft. maximum (floor-to-ceiling)
FENESTRATION	
Required fenestration area (% total street facade) on ground floor	80% of total ground floor facade area
PARKING REQUIREMENTS	
Number of required parking spots per sq. ft. of commercial	1 spot / 300 sqft commercial
SIGNAGE	
Allowed signage types	Surface or projecting
Allowed signage heights	Between 8 - 12 ft.



COMMERCIAL: TRAVELER DISTRICT 1B | Building Siting and Massing

ALLOWED LAND USES

Ground floor	Hotel Commercial
Upper floor(s)	Hotel Commercial

SETBACKS

From street front on ground floor	6 ft. maximum
From street front on upper floor(s)	0 ft. maximum
From side property lines on ground floor	0 ft. maximum
From side property lines on upper floor(s)	0 ft. maximum
From back property line on ground floor	20 ft. maximum
From back property line on upper floor(s)	20 ft. maximum

BUILD-TO-LINES

On street front on ground floor	None
On street front on upper floor(s)	Street front property line = build-to-line
On side property lines on ground floor	None
On side property lines on upper floor(s)	None
On back property line on ground floor	None
On back property line on upper floor(s)	None

BUILDING HEIGHTS

Ground floor (minimum and maximum)	10 ft. minimum, 12 ft. maximum (floor-to-ceiling)
Upper floor(s) (minimum and maximum)	9 ft. minimum, 11 ft. maximum (floor-to-ceiling)

FENESTRATION

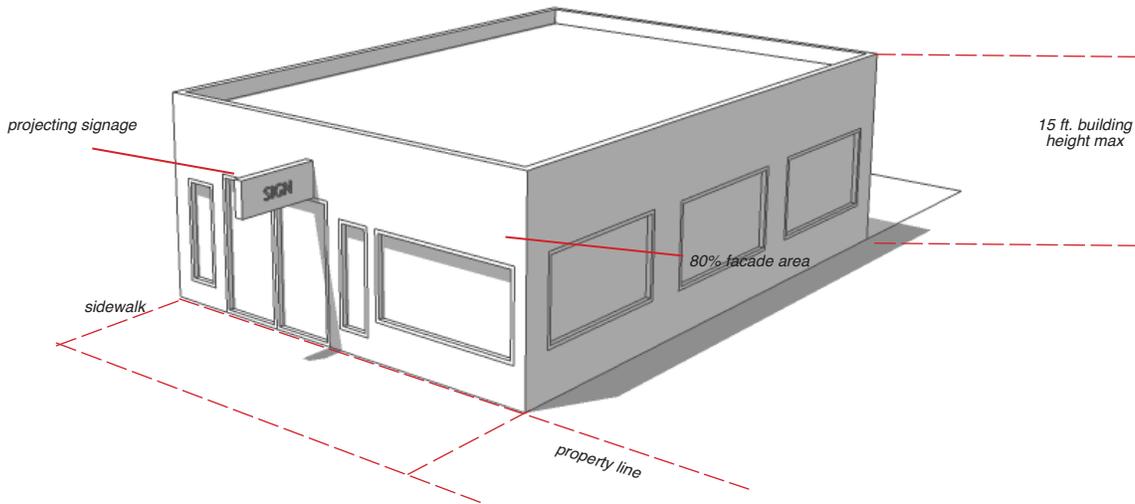
Required fenestration area (% total street facade) on ground floor	80% of total ground floor facade area
Required fenestration area (% total street facade) on upper floor(s)	50% of total ground floor facade area

PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of Hotel Commercial	1 spot / hotel room
------------------------------------------------------------------	---------------------

SIGNAGE

Allowed signage types	Surface or projecting
Allowed signage heights	Between 8 - 12 ft.



COMMERCIAL: TRAVELER DISTRICT 1C | Building Siting and Massing

ALLOWED LAND USES

Ground floor Gas Station

SETBACKS

From street front on ground floor 6 ft. maximum
 From side property lines on ground floor 0 ft. maximum
 From back property line on ground floor 100 ft. minimum

BUILD-TO-LINES

On street front on ground floor None
 On side property lines on ground floor Building should meet at least one property line
 On back property line on ground floor None

BUILDING HEIGHTS

Ground floor (minimum and maximum) 10 ft. minimum, 12 ft. maximum (floor-to-ceiling)

FENESTRATION

Required fenestration area (% total street facade) on ground floor 80% of total ground floor facade area

PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of commercial: gas station 1 spot / 300 sqft commercial

SIGNAGE

Allowed signage types Surface, projecting or hanging from cantilever
 Allowed signage heights Between 8 - 12 ft.



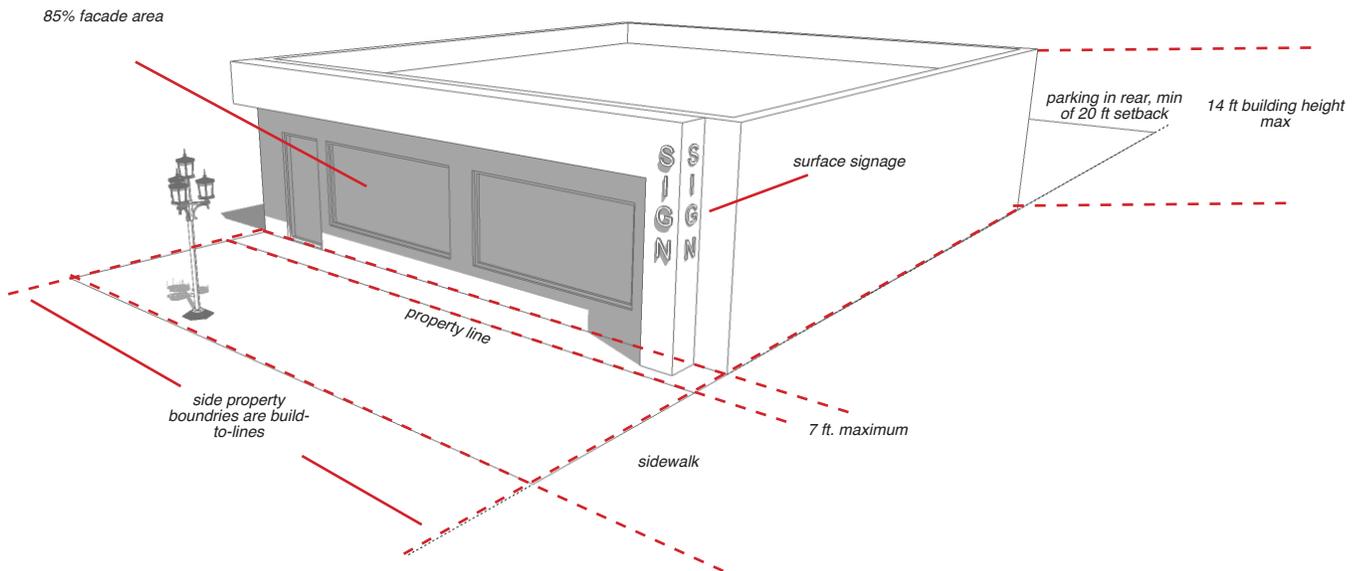
Name of Regulation Area: COMMERCIAL: NIGHTLIFE

Urban Design Objectives for the Regulation Area:

One of the most important aspects of the redevelopment of the Avenue of Flags is the emphasis on broad ranges of commercial use. One of the largest commercial uses that is intended for Buellton is the presence of a nightlife commercial district; a busy place capable of attracting visitors to the town during the night hours. This commercial zone would be the busiest of the entire project, it will invite the after-5 crowd, that the people of Buellton have showed increased interest in.

The commercial nightlife district focuses on three main goals:

- 1. To provide a fun and engaging area for crowds of all ages, with 18-21+ attractions becoming the spotlight activates during evening hours.*
- 2. To revamp economic interests in Buellton, and to provide a sustainable and attractive appeal for investors and store owners.*



COMMERCIAL: NIGHTLIFE I Building Siting and Massing

ALLOWED LAND USES

Ground floor	Commercial (Nightlife)
Upper floor(s)	Commercial, but only allowed on back parcels

SETBACKS

From street front on ground floor	7 ft maximum
From street front on upper floor(s)	0 ft maximum
From side property lines on ground floor	0 ft maximum
From side property lines on upper floor(s)	0 ft maximum
From back property line on ground floor	20 ft minimum, 30 ft maximum
From back property line on upper floor(s)	20 ft minimum, 30 ft maximum

BUILD-TO-LINES

On street front on ground floor	None
On street front on upper floor(s)	Property line = built-to-lines
On side property lines on ground floor	Property line = built-to-lines
On side property lines on upper floor(s)	Property line = built-to-lines
On back property line on ground floor	None
On back property line on upper floor(s)	None

BUILDING HEIGHTS

Ground floor (minimum and maximum)	12 ft minimum, 15 ft maximum (floor to ceiling)
Upper floor(s) (minimum and maximum)	12 ft minimum, 15 ft maximum (floor to ceiling)

FENESTRATION

Required fenestration area (% total street facade) on ground floor	85% of total floor facade area
Required fenestration area (% total street facade) on upper floor(s)	90% of total floor facade area

PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of commercial	Commercial - 1 spot every 300 sqft
------------------------------------------------------------	------------------------------------

SIGNAGE

Allowed signage types	Surface, projected, beveled, or hanging
Allowed signage heights	Between 7 ft and 12 ft, 1-12 ft if beveled and vertical.



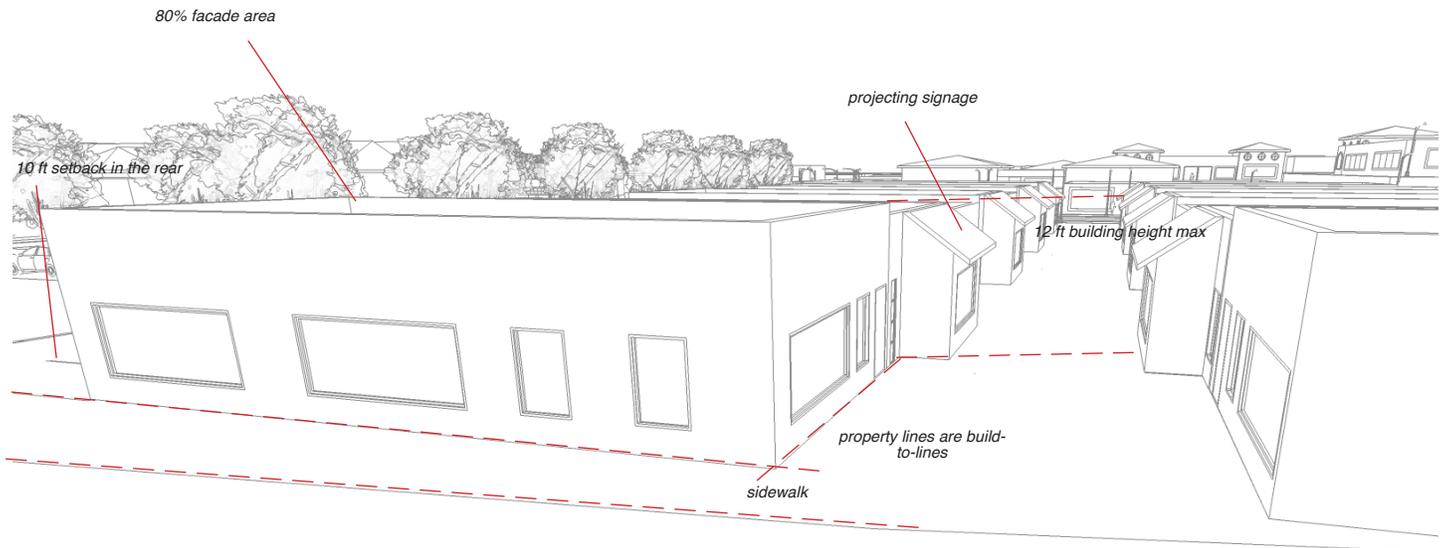
Name of Regulation Area: COMMERCIAL: FAMILY

Urban Design Objectives for the Regulation Area:

The commercial uses defined in this part of the FBC is intended to support the activities that would take place in the Family-Oriented Commercial area. Throughout the community meetings in Buelton, the residents have emphasized that there is a need for not only to attract people to the Avenue, but also to encourage them to “linger,” spending time in the project area.

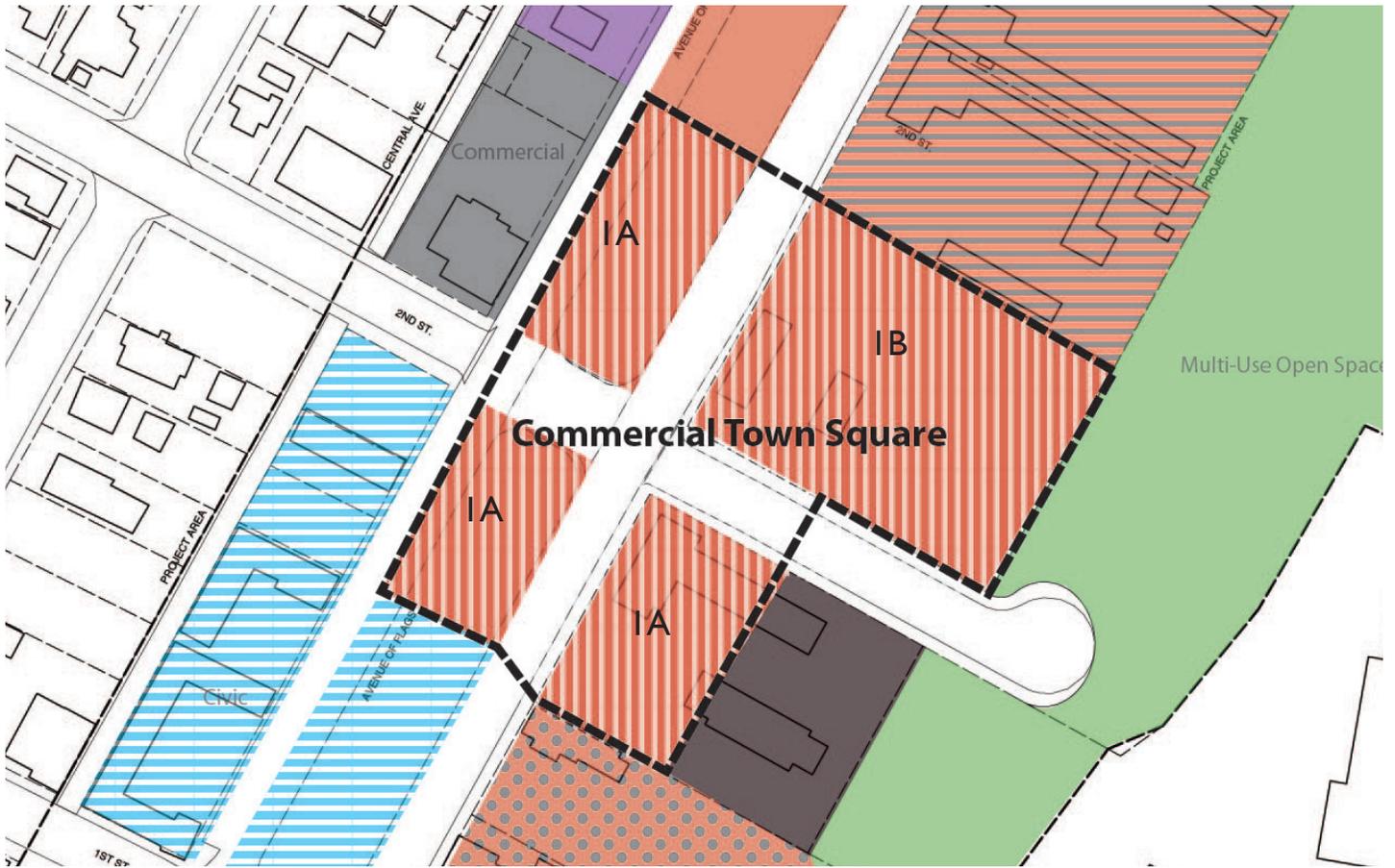
The urban design team objective in proposing this mixed-use area is threefold:

- 1. To provide ample shopping and dining opportunities to families by creating commercial services.*
- 2. To create a friendly, walkable, family-oriented environment by providing commercial amenities.*
- 3. To provide commercial establishments appropriate for people of all age groups.*



FAMILY-ORIENTED COMMERCIAL I Building Siting and Massing

ALLOWED LAND USES	
Ground floor	Commercial
PROPERTY LOCATION GUIDELINES	
Units that will be designed on these two parcels should face a walkway perpendicular to Avenue of Flags.	
The walkway should be 20-30 ft. wide and should be built on easements of 10-15 ft. from the shared property line between the two parcels.	
BUILDING HEIGHTS	
Ground floor (minimum and maximum)	12-15 ft
FENESTRATION	
Required fenestration area (% total street facade) on ground floor	Front: 80%, Back: 80%, Sides: 80%
PARKING REQUIREMENTS	
Number of required parking spots per sq. ft. of USE 1 (specify)	1 spot / 300 sqft of commercial
SIGNAGE	
Allowed signage types	Projecting signage
Allowed signage heights	8-12 ft



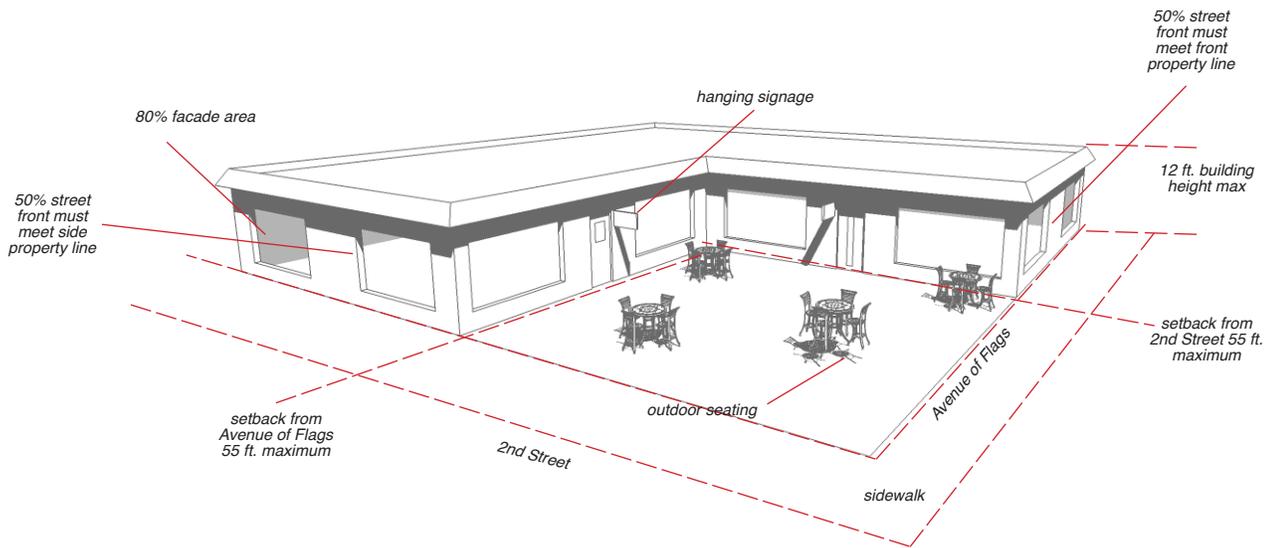
Name of Regulation Area: COMMERCIAL: TOWN SQUARE

Urban Design Objectives for the Regulation Area:

The commercial area defined in this part of the FBC is intended to support the activities that would take place in the Town Square area. Throughout the community meetings in Buelton, the residents have emphasized that there is a need for not only to attract people to the Avenue, but also to encourage them to “linger,” spending time in the project area.

The urban design team’s objectives in proposing this Town Square area is:

- 1. To provide a central area to come to and linger, by creating commercial services within a town square.*
- 2. To create a friendly, walkable, and open environment by providing four-way crosswalks to provide direct access to every commercial amenity in the Town Square area.*
- 3. To provide commercial establishments appropriate for people of all age groups and to keep people coming back.*



COMMERCIAL: TOWN SQUARE 1A | Building Siting and Massing

ALLOWED LAND USES

Ground floor	Commercial
Upper floor(s)	None

SETBACKS

From street front on ground floor	Maximum 55 ft. from Avenue of Flags and 2nd Street intersections, Minimum 50% of street front must meet property line on Avenue of Flags.
From street front on upper floor(s)	None
From side property lines on ground floor	Maximum 55ft. from Avenue of Flags and 2nd Street intersections, Minimum 50% of street front must meet property line on 2nd Street.
From side property lines on upper floor(s)	0 ft
From back property line on ground floor	0 ft
From back property line on upper floor(s)	0 ft

BUILD-TO-LINES

On street front on ground floor	Please refer to setback guidance above
On street front on upper floor(s)	None
On side property lines on ground floor	Please refer to setback guidance above
On side property lines on upper floor(s)	None
On back property line on ground floor	Please refer to setback guidance above
On back property line on upper floor(s)	None

BUILDING HEIGHTS

Ground floor (minimum and maximum)	12-15 feet
Upper floor(s) (minimum and maximum)	0 feet

FENESTRATION

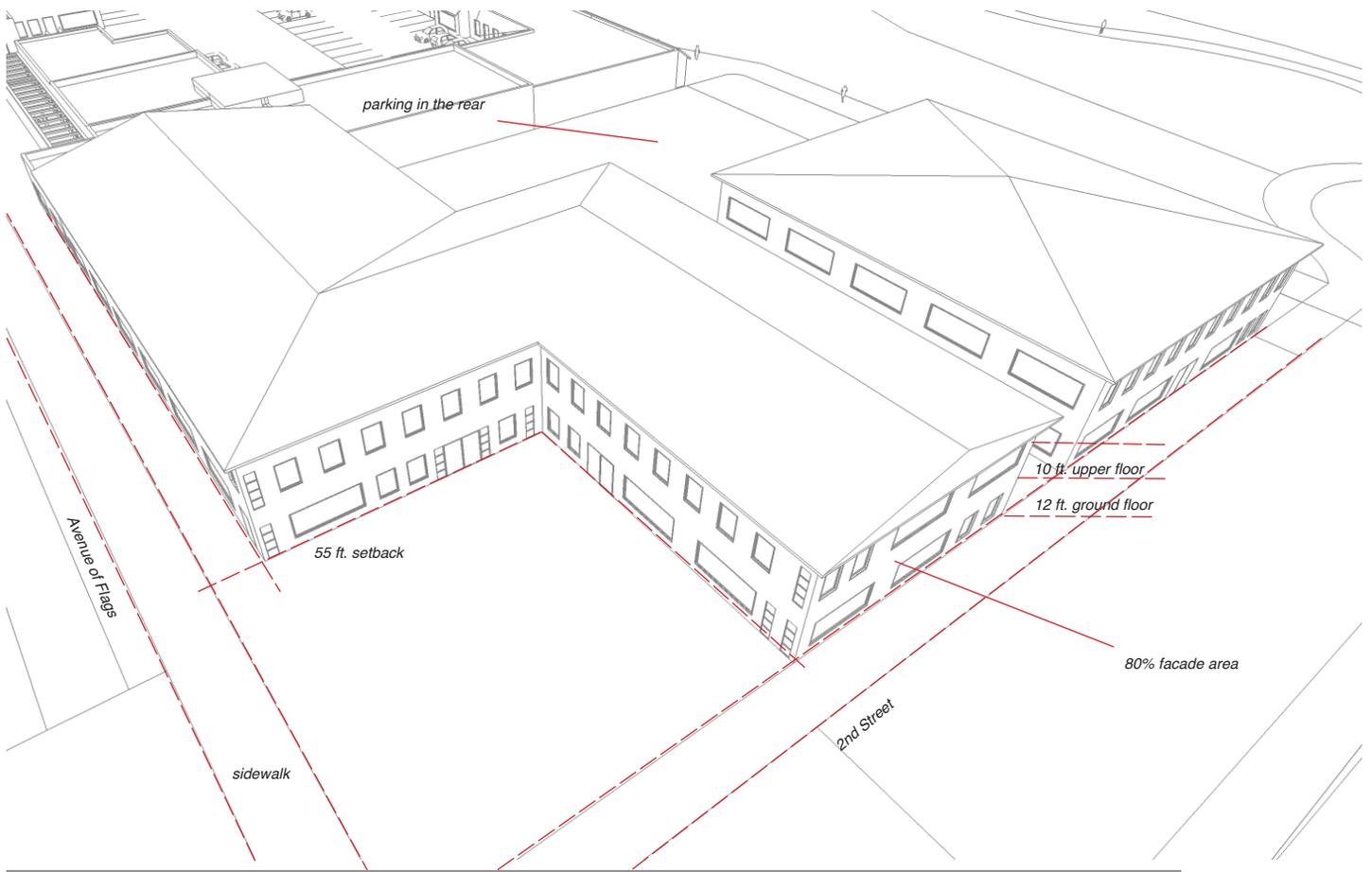
Required fenestration area (% total street facade) on ground floor	80% of total facade on all sides
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PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of commercial	No parking required next to public parking
------------------------------------------------------------	--------------------------------------------

SIGNAGE

Allowed signage types	Surface or hanging sign
Allowed signage heights	8-12 feet



COMMERCIAL: TOWN SQUARE 1B | Building Siting and Massing

ALLOWED LAND USES

Ground floor	Hotel Commercial
Upper floor(s)	Hotel Commercial

SETBACKS	
From street front on ground floor	Maximum 55ft. from Avenue of Flags and 2nd Street intersections, Minimum 50% of street front must meet property line on Avenue of Flags.
From street front on upper floor(s)	Refer to street front on ground floor
From side property lines on ground floor	Maximum 55ft. from Avenue of Flags and 2nd Street intersections, Minimum 50% of street front must meet property line on 2nd Street.
From side property lines on upper floor(s)	Refer to side property lines on ground floor
From back property line on ground floor	Minimum 60 ft. from back property line (open space) to accomodate near parking. Parking access should be provided from 2nd Street. Minimum 30% of rear facade should meet back property line (open space).
From back property line on upper floor(s)	Refer to back property line on ground floor
BUILD-TO-LINES	
On street front on ground floor	Please refer to setback guidance above
On street front on upper floor(s)	Please refer to setback guidance above
On side property lines on ground floor	Please refer to setback guidance above
On side property lines on upper floor(s)	Please refer to setback guidance above
On back property line on ground floor	Please refer to setback guidance above
On back property line on upper floor(s)	Please refer to setback guidance above
BUILDING HEIGHTS	
Ground floor (minimum and maximum)	12-15 feet
Upper floor(s) (minimum and maximum)	10-12 feet
FENESTRATION	
Required fenestration area (% total street facade) on ground floor	50-60% of total facade on all sides
Required fenestration area (% total street facade) on upper floor(s)	50-60% of total facade on all sides
PARKING REQUIREMENTS	
Number of required parking spots per sq. ft. of USE 1 (specify)	1 spot per room
SIGNAGE	
Allowed signage types	Surface or hanging
Allowed signage heights	8-12 feet



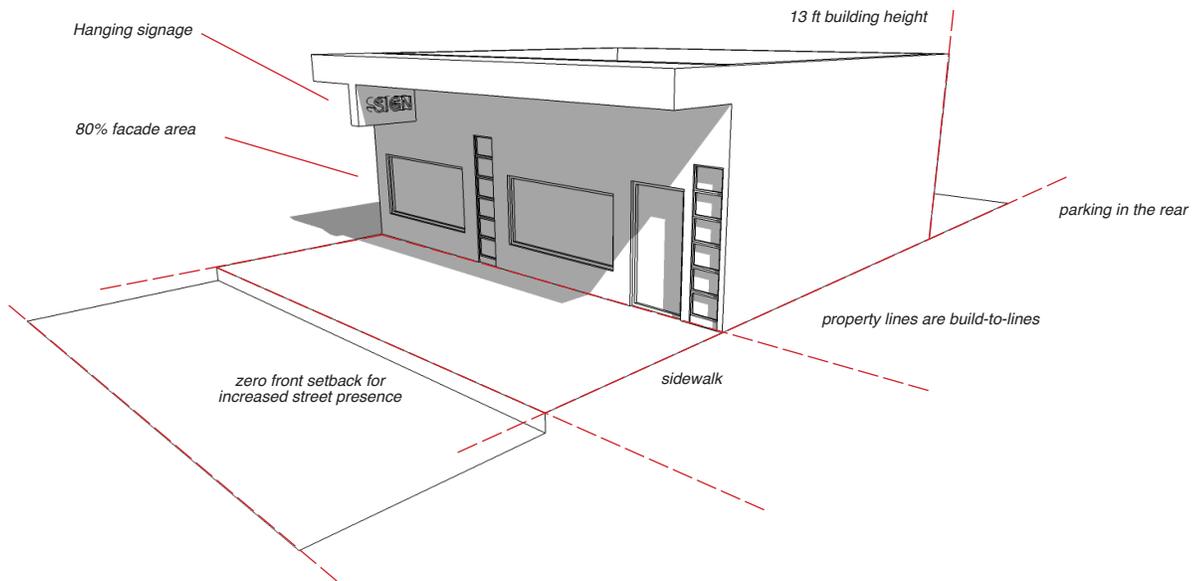
Name of Regulation Area: RETAIL COMMERCIAL

Urban Design Objectives for the Regulation Area:

The retail commercial district defined in this section of the FBC is intended to support retail business to fulfill local residents needs. Throughout the community meetings in Buelton, the residents expressed the need for more retail opportunities to increase visits to the Avenue as well as stimulate the local economy.

The urban design team's objective in proposing this commercial area is threefold:

- 1. To attract visitors to the area by providing retail shops in a pedestrian oriented environment.*
- 2. To increase local economic activity for Buleton by providing commercial services and shops.*
- 3. To give residents ample retail opportunities in one area of the site so visitors can fulfill their shopping needs.*



RETAIL COMMERCIAL DISTRICT I Building Siting and Massing

ALLOWED LAND USES	
Ground floor	Commercial
SETBACKS	
From street front on ground floor	0 feet
From side property lines on ground floor	0 feet
From back property line on ground floor	0 feet
BUILD-TO-LINES	
On street front on ground floor	None
On side property lines on ground floor	None
On back property line on ground floor	None
BUILDING HEIGHTS	
Ground floor (minimum and maximum)	12-15 feet
FENESTRATION	
Required fenestration area (% total street facade) on ground floor	80% of total ground floor facade area
PARKING REQUIREMENTS	
Number of required parking spots per sq. ft. of USE 1 (specify)	1 spot /300 sq. feet of commercial
SIGNAGE	
Allowed signage types	Hanging signs
Allowed signage heights	8-12 feet



Name of Regulation Area: MIXED-USE

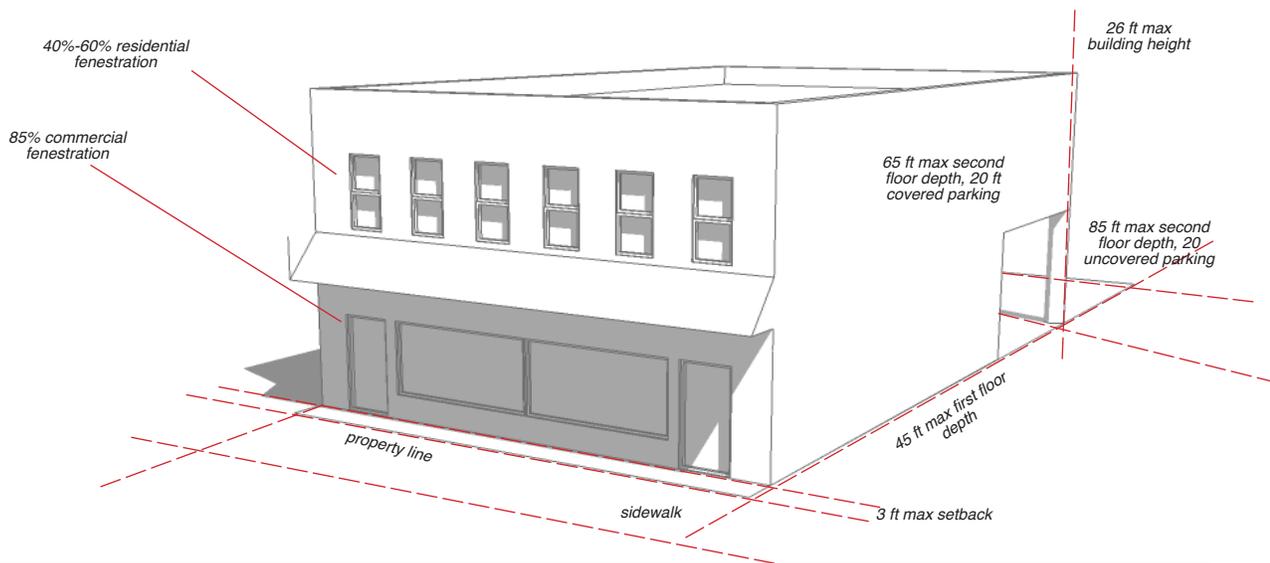
Urban Design Objectives for the Regulation Area:

The mixed-use developments on the Avenue of Flags are being proposed to help add a larger variety of uses to the project site, as well as to incorporate and introduce a more compact feel for the pedestrian walkways. By placing commercial units in close proximity to residential units, live/work relationships can form that have the potential to boost and encourage economic growth and a more local feel.

The proposal of mixed-use developments in the Avenue of Flags project site serves two goals:

1. To expand and broaden the types of developments within the project site. An increase in diversity has the potential to increase interest and development of an area.

2. To promote live/work environments. Creating a local, convenient, and more personal environment for the pedestrian and commercial areas of Buellton is a large priority in this project.



MIXED-USE I Building Siting and Massing

ALLOWED LAND USES

Ground floor	Commercial (Neighborhood)
Upper floor(s)	Residential (Apartments)

SETBACKS

From street front on ground floor	6 ft maximum
From street front on upper floor(s)	0 ft maximum
From side property lines on ground floor	0 ft maximum
From side property lines on upper floor(s)	0 ft maximum
From back property line on ground floor	40 ft minimum
From back property line on upper floor(s)	20 ft minimum

BUILD-TO-LINES

On street front on ground floor	0 ft maximum
On street front on upper floor(s)	Property line = built-to-lines
On side property lines on ground floor	Property line = built-to-lines
On side property lines on upper floor(s)	Property line = built-to-lines
On back property line on ground floor	None
On back property line on upper floor(s)	None

BUILDING HEIGHTS

Ground floor (minimum and maximum)	12 ft minimum/maximum (floor to ceiling)
Upper floor(s) (minimum and maximum)	12ft minimum, 14 ft maximum (floor to ceiling)

FENESTRATION

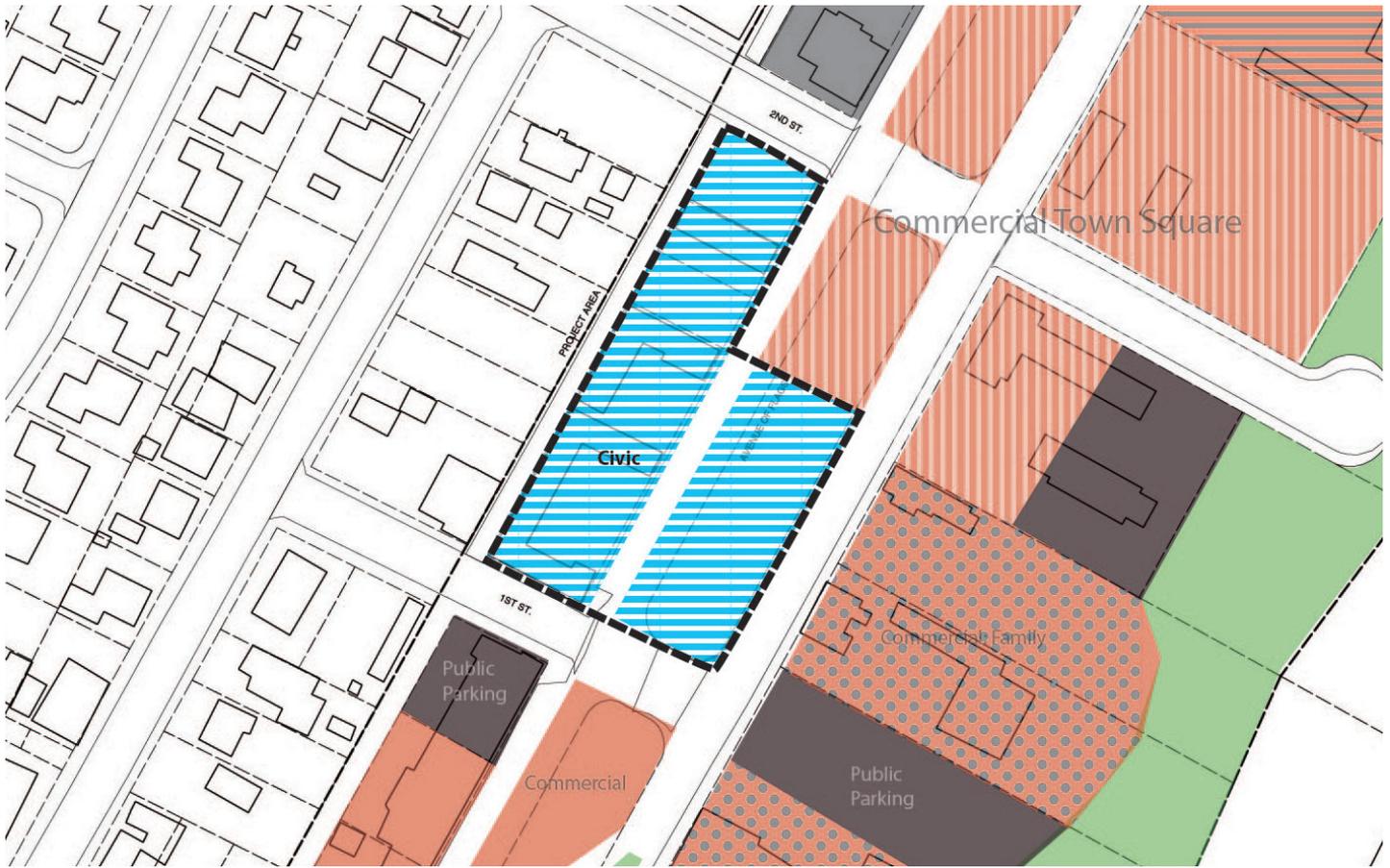
Required fenestration area (% total street facade) on ground floor	85% of total floor facade area
Required fenestration area (% total street facade) on upper floor(s)	40%-60% of total floor facade area

PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of USE 1 (specify)	Commercial - 1 spot every 360 sqft
Number of required parking spots per sq. ft. of USE 2 (specify)	Residential - 1 spot per BR, 1/2 spot per additional BR

SIGNAGE

Allowed signage types	Surface, must be projected or hanging
Allowed signage heights	Between 8 ft and 12 ft



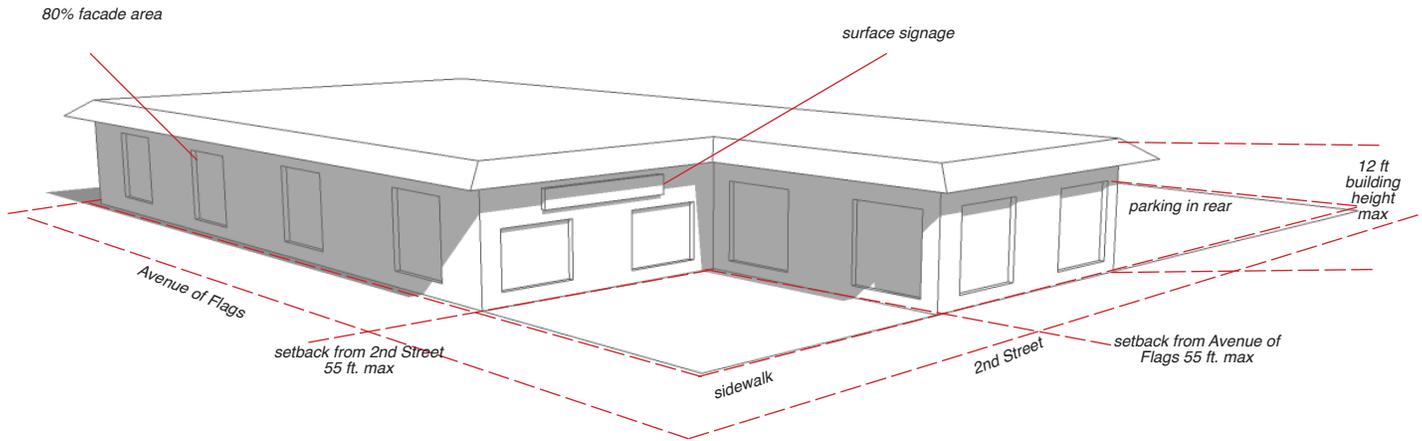
Name of Regulation Area: CIVIC

Urban Design Objectives for the Regulation Area:

The civic use defined in this part of the FBC is intended to support the activities that would take place in a public area or civic center. Throughout the community meetings in Buelton, the residents have emphasized that there is a need for not only to attract people to the Avenue, but also to keep the local community fully involved in the growing Avenue.

The urban design team's objective in proposing this civic area is:

- 1. To provide ample space for community center, library, Buelton history museum, or other civic needs of the community.*
- 2. To create a friendly, inviting environment for the locals host community events or to come use the civic use that the area will hold.*
- 3. To provide community establishments appropriate for people of all age groups.*



CIVIC I Building Siting and Massing

ALLOWED LAND USES

Ground floor	Civic
Upper floor(s)	

SETBACKS

From street front on ground floor	Maximum 55 ft. from Avenue of Flags and 2nd Street intersections, Minimum 50% of street front must meet property line on Avenue of Flags.
From street front on upper floor(s)	None
From side property lines on ground floor	Maximum 55ft. from Avenue of Flags and 2nd Street intersections, Minimum 50% of street front must meet property line on 2nd Street.
From side property lines on upper floor(s)	0 feet
From back property line on ground floor	0 feet
From back property line on upper floor(s)	0 feet

BUILD-TO-LINES

On street front on ground floor	Please refer to setback guidance above
On street front on upper floor(s)	None
On side property lines on ground floor	Please refer to setback guidance above
On side property lines on upper floor(s)	None
On back property line on ground floor	Please refer to setback guidance above
On back property line on upper floor(s)	None

BUILDING HEIGHTS

Ground floor (minimum and maximum)	12-15 feet
Upper floor(s) (minimum and maximum)	None

FENESTRATION

Required fenestration area (% total street facade) on ground floor	80% of total facade on all sides
Required fenestration area (% total street facade) on upper floor(s)	0%

PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of USE 1 (specify)	1 spot per 300 sq. feet
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SIGNAGE

Allowed signage types	Surface
Allowed signage heights	8-12 feet



Name of Regulation Area: MULTI-USE OPEN SPACE

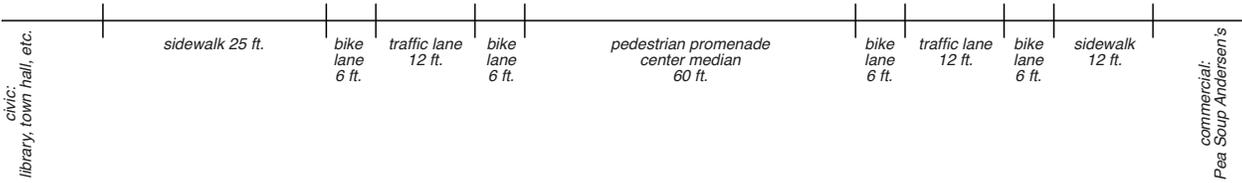
Urban Design Objectives for the Regulation Area:

The multi-use open space defined in this part of the FBC is intended to support the activities that would take place in the proposed Nightlife Commercial and Family Commercial area. The residents of Buellton have emphasized the need for open space and various activities incorporated into the open area. The team is proposing an outdoor amphitheatre located north of 2nd Street, and a children's water feature and dog park south of 2nd Street.

The urban design team's objective in proposing this

multi-use open space is threefold:

1. To increase the outdoor activity by providing a pedestrian and bicycle path through the open space.
2. To provide for different age groups with daytime and nighttime entertainment with an outdoor amphitheatre, children's water feature, and dog park.
3. To increase the significance of the monument by relocating it at the end of the pathway through the open space.



Avenue of Flags (AoF) | Streetscaping from Highway 246 to 1st street

TRAFFIC LANES

Direction 1, number and width	Northbound, 12 ft
Direction 2, number and width	Southbound, 12 ft

BIKE LANES

Direction 1, width	Northbound, 6ft
Direction 2, width	Southbound, 6ft

SIDEWALKS

Direction 1, width	Northbound, 12ft
Direction 2, width	Southbound, 12ft

MEDIAN (IF AVAILABLE)

Width	40 ft, reduces to 10ft at north edge
Other properties	Public art area, grassy

STREET TREES

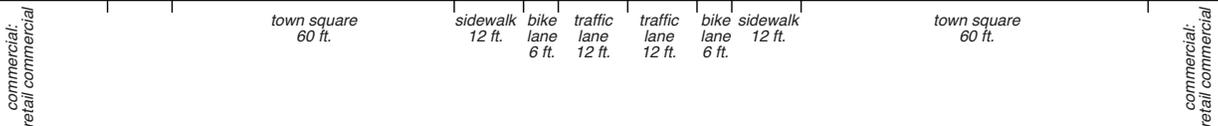
Direction 1, type	Northbound, Japanese Pagoda Tree (<i>Sophora Japonica</i>), Flaxleaf Paperback (<i>Melaleuca Linerifolia</i>), Flouring Pear (<i>Pyrus Calleryana</i>)
Direction 1, frequency	Every 60ft with no set pattern, no more than 2 similar trees can be placed together.
Direction 2, type	Southbound, Japanese Pagoda Tree (<i>Sophora Japonica</i>), Flaxleaf Paperback (<i>Melaleuca Linerifolia</i>), Flouring Pear (<i>Pyrus Calleryana</i>)
Direction 2, frequency	Every 60ft with no set pattern, no more than 2 similar trees can be placed together.

STREET LIGHTING

Type 1, purpose and frequency	Gaslamp style laterns, increase aesthetic appeal and lighting, 80ft
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RECOMMENDED LOW-IMPACT DEVELOPMENT STRATEGIES

Technique	Explanation
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Avenue of Flags (AoF) | Streetscaping from 1st Street to 2nd Street

TRAFFIC LANES

Direction 1, number and width	Northbound, 12 ft
Direction 2, number and width	Southbound, 12 ft

BIKE LANES

Direction 1, width	Northbound, 6ft
Direction 2, width	Southbound, 6ft

SIDEWALKS

Direction 1, width	Northbound, 12ft
Direction 2, width	Southbound, 12ft

STREET TREES

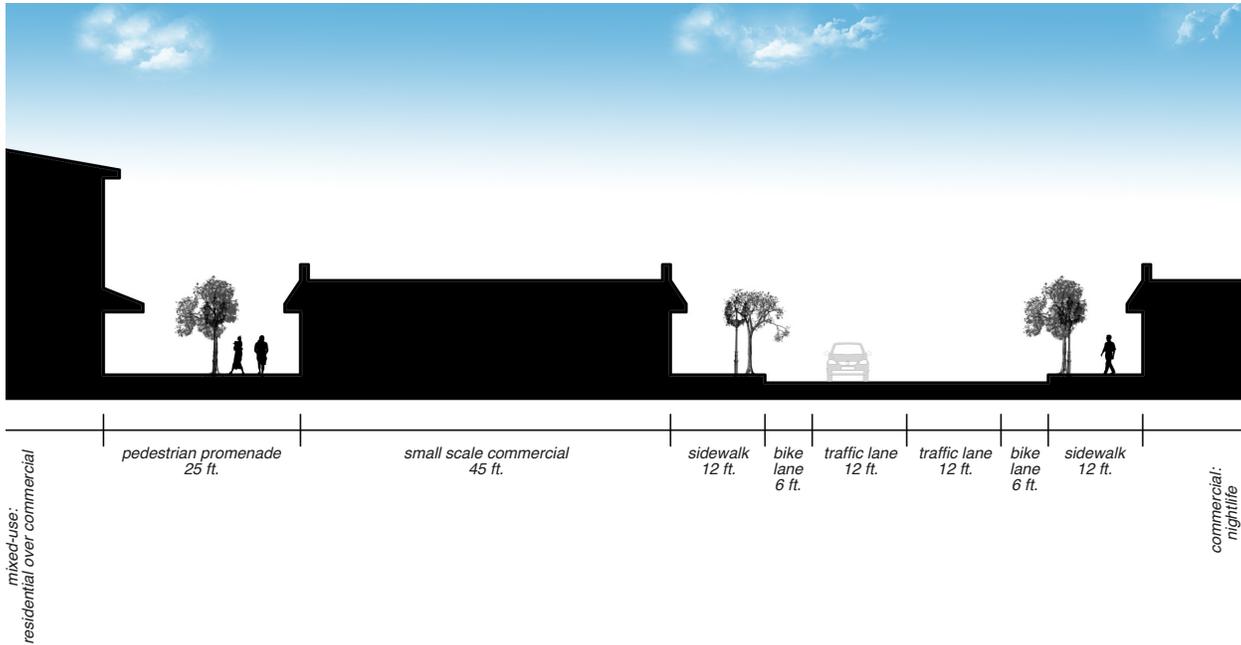
Direction 1, type	Northbound, Japanese Pagoda Tree (<i>Sophora Japonica</i>), Flaxleaf Paperback (<i>Melaleuca Linerifolia</i>), Flouing Pear (<i>Pyrus Calleryana</i>)
Direction 1, frequency	Every 60ft with no set pattern, no more than 2 similar trees can be placed together.
Direction 2, type	Southbound, Japanese Pagoda Tree (<i>Sophora Japonica</i>), Flaxleaf Paperback (<i>Melaleuca Linerifolia</i>), Flouing Pear (<i>Pyrus Calleryana</i>)
Direction 2, frequency	Every 60ft with no set pattern, no more than 2 similar trees can be placed together.

STREET LIGHTING

Type 1, purpose and frequency	Gaslamp style laterns to increase aesthetic appeal and lighting every 80ft
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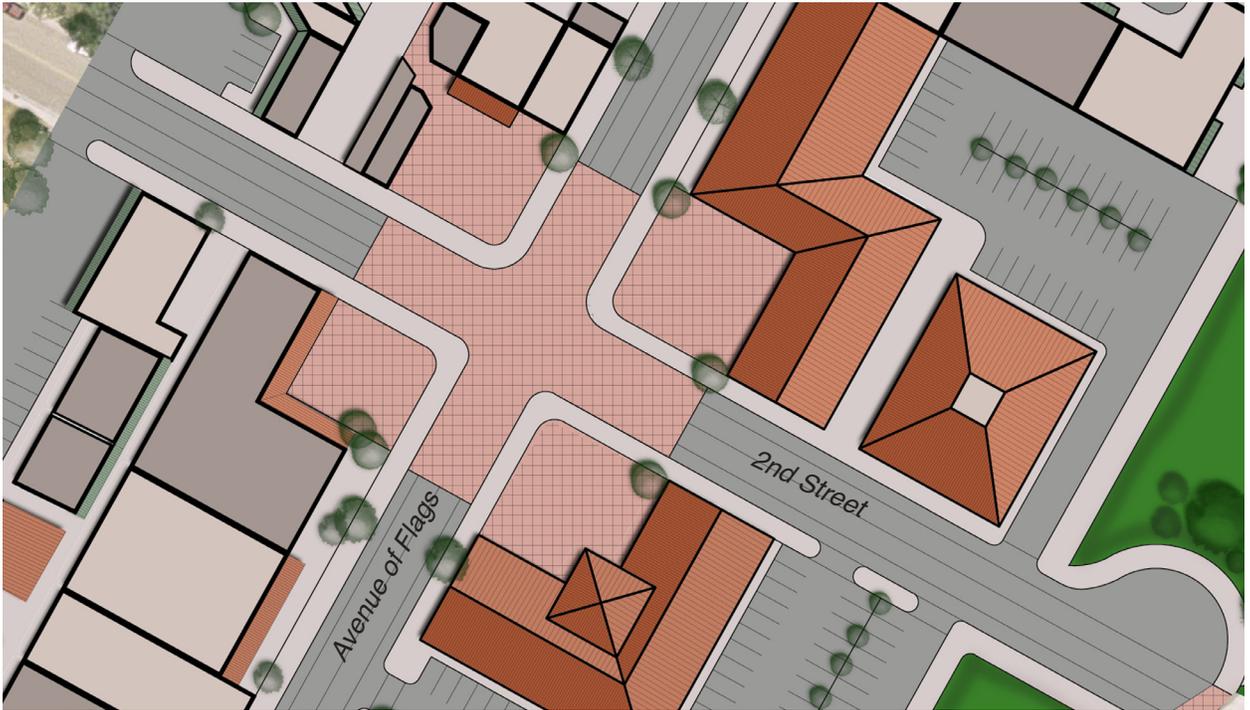
RECOMMENDED LOW-IMPACT DEVELOPMENT STRATEGIES

Technique	Explanation
Raised and textured pavement	Traffic calming and pedestrian friendly



Avenue of Flags (AoF) I Streetscaping from 2nd Street to Damassa Road

TRAFFIC LANES	
Direction 1, number and width	Northbound, 12 ft
Direction 2, number and width	Southbound, 12 ft
BIKE LANES	
Direction 1, width	Northbound, 6ft
Direction 2, width	Southbound, 6ft
SIDEWALKS	
Direction 1, width	Northbound, 12ft
Direction 2, width	Southbound, 12ft
STREET TREES	
Direction 1, type	Northbound, Japanese Pagoda Tree (Sophora Japonica), Flaxleaf Paperback (Melaleuca Linerlifolia), Flouing Pear (Pyrus Calleryana)
Direction 1, frequency	Every 60ft with no set pattern, no more than 2 similar trees can be placed together.
Direction 2, type	Southbound, Japanese Pagoda Tree (Sophora Japonica), Flaxleaf Paperback (Melaleuca Linerlifolia), Flouing Pear (Pyrus Calleryana)
Direction 2, frequency	Every 60ft with no set pattern, no more than 2 similar trees can be placed together.
STREET LIGHTING	
Type 1, purpose and frequency	Gaslamp style lanterns to increase aesthetic appeal and lighting every 80ft



Intersection

Avenue of Flags and 2nd Street

TRAFFIC CALMING MEASURE(S)

Traffic calming type

Raised textured intersection

The intersection should be raised and textured to calm traffic and maximize pedestrian safety by allowing complete pedestrian access through the intersection.

PEDESTRIAN CROSSINGS

Direction: North-south, East side

Width 30'

Material(s) Brick

Direction: North-south, West side

Width 30'

Material(s) Brick

Direction: East-west, North side

Width 30'

Material(s) Brick

Direction: East-west, South side

Width 30'

Material(s) Brick

Notes: Allows for diagonal crossing with signaled lighting.

Textured ground extends beyond pedestrian crossing 60' to create the town square atmosphere.



Intersection Avenue of Flags and 1st Street

TRAFFIC CALMING MEASURE(S)

Traffic calming type

Textured Crosswalks

Proposed on the three streets to calm traffic and maximize pedestrian access and safety.

PEDESTRIAN CROSSINGS

Direction: North-South, East side

Width 10'

Material(s) Brick

Direction: North-South, West side

Width 10'

Material(s) Brick

Direction: East-West, South side

Width 10'

Material(s) Brick

Direction: East-West, North side

Width 10'

Material(s) Brick

Notes:



Intersection Avenue of Flags and Damassa Road

TRAFFIC CALMING MEASURE(S)

Traffic calming type	
Traffic Circle	The traffic circle is proposed to be 70' in diameter with a 20' single lane around the center circular median and the cobblestone or brick area so trucks can turn safely.

PEDESTRIAN CROSSINGS

Direction:	North-South, East Side
Width	10'
Material(s)	Brick
Direction:	North-South, West Side
Width	10'
Material(s)	Brick
Direction:	East-West, South side
Width	10'
Material(s)	Brick
Direction:	East-West, North side
Width	10'
Material(s)	Brick
Notes:	



Tree box filters



Permeable grass pavers for Multi-Use Open Space



Permeable pavers for Town Square Intersection

PUBLIC PROPERTIES | Low-impact Development Guidelines

Technique	Explanation
Permeable pavers for Town Square intersection	Allows the movement of stormwater around paving, to infiltrate through areas that would normally be impervious to the soil below.
Permeable grass pavers for Multi-Use Open Space	Allows people or cars move smoothly over the surface, but also allows stormwater to reach the soil too.
Tree box filters	Filters are a soft-engineered stormwater management approach designed to collect, treat, and percolate stormwater runoff.



Planters with trees in parking lots



Green roofs

PRIVATE | Low-impact Development Guidelines

STRUCTURES: RECOMMENDED LOW-IMPACT DEVELOPMENT STRATEGIES

Technique	Explanation
Green roofs	Roof of building that is partially or completely covered with vegetation and a growing medium, planted over a waterproofing membrane. It absorbs rainwater, provides insulation, and helps lower air temperature.

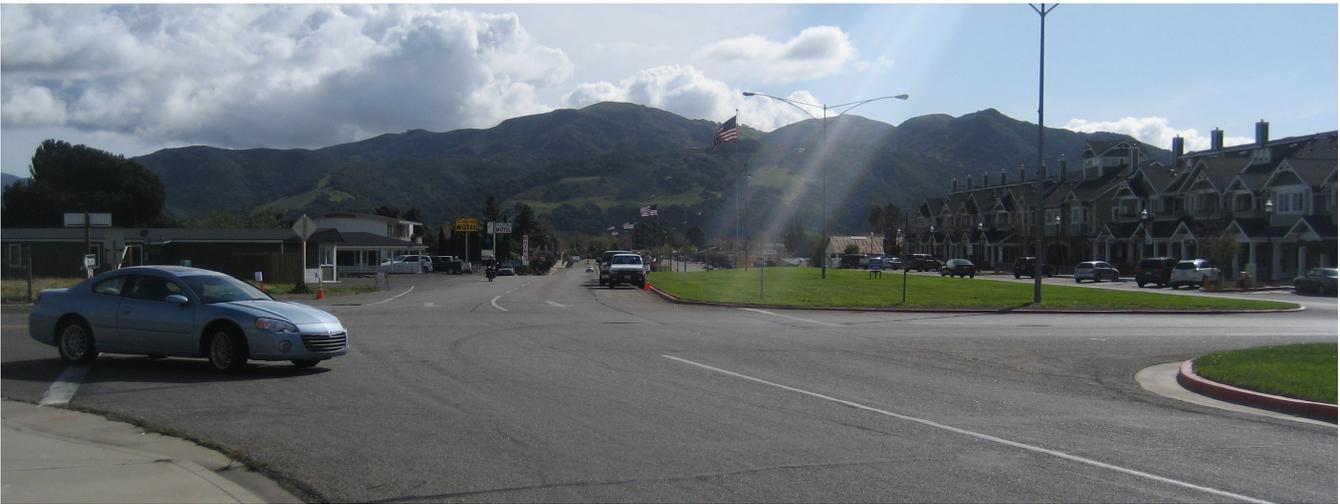
LANDSCAPING: RECOMMENDED LOW-IMPACT DEVELOPMENT STRATEGIES

Planters with trees in parking lots	Provides shade for cars and people, reduces paved areas, and has environmental benefits.
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illustrative site plan (figure 5.3)



3D Still Renderings



Before: The existing freeway ramp for Highway 101 at the north end of the project site.



After: Avenue of Flags proposed traffic calming strategy and traveler district.



Before: Avenue of Flags existing mixed-use buildings with street front parking.



After: Avenue of Flags existing mixed-use buildings with pedestrian promenade and new commercial development.



Before: Existing open space on the project site.

After: Proposed multi-use open space with bike path and amphitheater.





Before: Existing avenue with two-lane roads on either side of the center median.



After: Proposed town square and main two-lane road.



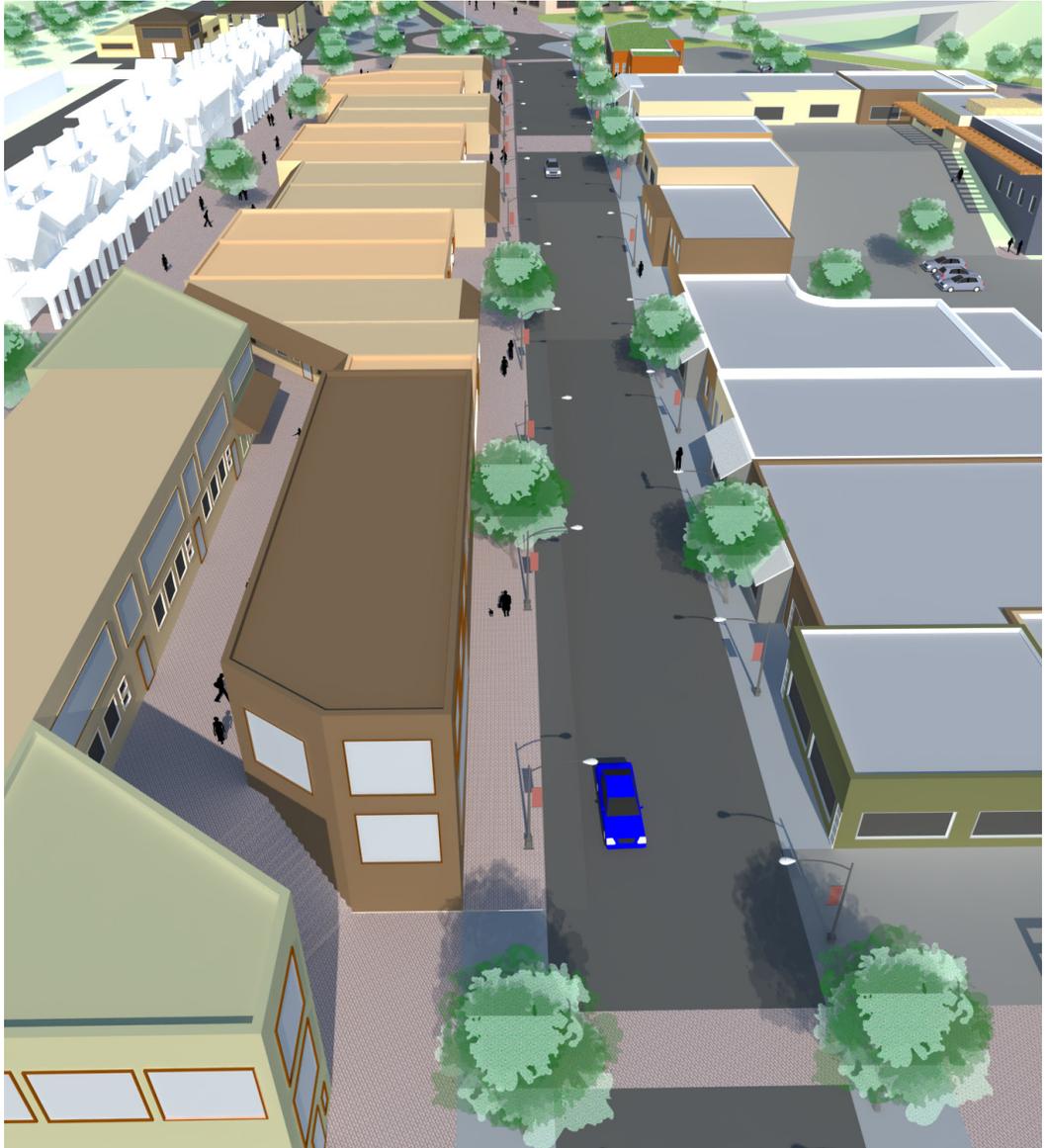
Before: Existing buildings adjacent to the open space.



After: Proposed nightlife commercial district adjacent to the multi-use open space.



Proposed family-oriented commercial district.



Proposed commercial use for the Avenue of Flags.

Proposed civic center for the Avenue of Flags.



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