

Avenue of Flags Buellton, CA

Urban Design Plan

CAL POLY
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1.1 Introduction

In the 2012 spring quarter, Cal Poly City and Regional Planning's Urban Design Studio II collaborated with the City of Buellton's Planning Department to develop urban design plan proposals for Avenue of Flags. The collaborative effort consisted of four meetings with community members and planning staff to ensure that plan proposals reflected the community members' wishes for the future of the area.

In the first community meeting, the Urban Design Studio met with the community members to visit Buellton. The community members and the Urban Design Studio went on an 'awareness walk' on and around Avenue of Flags. While on the walk, community members took pictures of what they like about the Avenue and what they would like to improve. The community members discussed their concerns about the avenue with the students. The Urban Design Studio was able to get a feeling for the scale, amenities, and the community sentiments about Avenue of Flags during the awareness walk. This helped the teams develop their conceptual diagrams for the future redevelopment of the avenue.

At community meeting II, the Urban Design Studio presented their conceptual design proposals to the community. Each team presented different conceptual diagrams with photos illustrating their conceptual ideas using examples from other cities. The community gave feedback about the diagrams and photos. The community members also classified in order of importance, their 'wishes' and their 'haves' for Avenue of Flags. This informed the team about what the community wants most and what they have already but want to maximize in the future. The community members also drew their own conceptual diagrams, designating where they thought land uses and infrastructure should be. Both of these exercises lead the students into their second iteration of site plans.

Community meeting III was held on the weekend. It presented the second phase in the team's design process to the community members. They informed the team about their thoughts on the design progress, which spurred the third iteration of the design.

Community meeting III reviewed the land use proposals and illustrative studies that the team developed. It also launched the development of the team's form-based-codes and phasing scheme for the avenue. In the meeting, community members placed "like" and "dislike" stickers on the land use proposals and illustrative plans on areas or elements they liked or thought needed improvement. The team provided more examples to support their ideas from other cities. The community also put stickers on the example photos to show what urban elements from other cities they thought would or would not fit into Avenue of Flags. From this last

meeting, the students and community had a strong understanding of the future needs for the avenue. The discussions and feedback helped guide the team in their forth iteration, which would result in an animation of the urban design proposal for Avenue of Flags.

On June 6, a Buellton council meeting was held so the teams could present their final urban design proposals to the Buellton Planning Commission and Council as well as the community members. In the meeting, the team gave a 15-minute presentation explaining the proposed land uses, circulation, form-based codes, and phasing strategy. They ended their presentation with a fly through animation of what the team envisions Avenue of Flags to look like post development. The presentation followed with a question and answer session between the audience and team members.

The following document discusses the team's urban design process and proposal. The second chapter provides background about Avenue of Flags. It describes the existing environment, land uses, and circulation. It also explains the relevant documents that helped assist the team in their development process. Chapter three discusses the team's conceptual development process. Chapter four explains the proposed land uses, community amenities, and circulation for Avenue of Flags. Chapter five illustrates the proposed form-based codes, and it also presents the proposed phasing plan.



Chapter 2 Site inventory and Analysis



2.1 The Site

Buellton is located in Santa Barbara County, California. The city is located 45 minutes above the City of Santa Barbara and 35 miles below the City of Santa Maria. It is located off Highway 101 and State Highway 246. The project area is located around the Avenue of Flags. This is a street in Buellton that is large and was Highway 101, however the highway was moved to its current location and now the Avenue of Flags is street that has a rather large medians.

The City of Buellton has a unique history. The area dates as far back as 1875, when Buell Ranch was a small town. The area contained a general store, a post office, bunkhouses, blacksmith shop and family homes. R.T. Buell was the owner of the ranch. He was born in Essex, Vermont and grew up in a farm. He went to school around the east coast, but decided to travel to California to try his luck in the gold rush. However, he was not successful in mining and decided to travel to Marin County where he started a successful dairy farm. After a while, he traveled down to the south to where they found land that was for sale in the Santa Ynez Valley. After a drought, R.T went into debt and sold his choicest part of the ranch to pay off his bills. (<http://www.cityofbuellton.com/history.asp>)

Another important family in the Buellton area is the de la Cuesta family. Dr. Roman de la Cuesta was born in Spain and came to California in 1849. He purchased the Rancho La Vega in 1851 which consisted of 8,000 acres. He had a large family and they were instrumental in getting Highway 101 through Buellton.

The area began to rapidly change with more settlers. In 1911, Danish settlers were spilling into the Buellton area. Stores were built and the Famous Pea Soup Andersons was placed there (See Figure 2.1). Buellton has always been a gate way in the Santa Ynez Valley. Avenue of Flags was built to be part of the highway 101, however, in 1960 they moved it to its current location. Buellton became an incorporated city in February 1,1992.

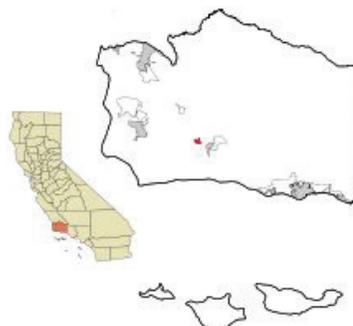
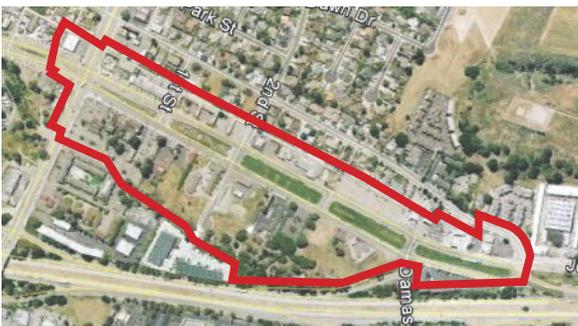


Figure 2.1 (Above): Anderson's Pea Soup photo (Source: <http://www.cityofbuellton.com/history.asp>)

Figure 2.2 (Left): Project site within Buellton; (Right): Location of Buellton within California (Source:)

2.2 Existing Land Uses on the Site

At the North end of the Avenue of Flags there is a new hotel and a renovated motel. Across the Avenue is a 76 gas station. This area is to the West of the on/off ramps to Highway 101. At the corner of Highway 246 and the Avenue of Flags is Pea Soup Anderson's, which has a restaurant and hotel for the traveler, and three other stores that cater to the locals. Just north of the 246 on the Avenue is a line of shops for locals, such as Farmers Insurance and a western wear store. The Eastern side of the Avenue of Flags exist motels, left over from when the 101 went through the city. On the Western side of the Avenue, there are a few empty lots and other businesses. There is also a new mixed-use complex that has businesses on the bottom and apartments on the second floor. Some of those apartments are affordable units.

Along the Avenue of Flags there are some opportunities that could help the area prosper. When Highway 101 was moved, it left a large amount of open space that provides a large amount of developable space. The Santa Ynez River flows to the South of Buellton and provides water for the town. There is also Zaca Creek, which runs parallel to the Avenue of Flags and past the old zoo property. There is a possibility to incorporate the old zoo property and the creek to the Avenue. Pea Soup Anderson's creates an opportunity for the project site by attracting visitors to Buellton.

There are also constraints for the project site. Highway 246 and Highway 101 have large amounts of truck traffic. The trucks generate a lot of noise and traffic along the Avenue of Flags. Even without the trucks traffic moves fast along the Avenue of Flags and Highway 246. Several of the motels on the Avenue are not well maintained. There are several empty lots and closed businesses that create a less inviting atmosphere. A major constraint this site faces is that Buellton has long been a place that is thought of as a transitory, service-town. It has all the services for the traveler however it has yet to be a destination town.

Figure 2.3: Motels on Avenue of Flags



Figure 2.4: Zaca Creek



Existing Land Uses Map



2.3 Circulation and Transportation

Intersection with Highway 246:

The Avenue of Flags is a former highway that runs through the center of Buellton running north-south. It is roughly two-hundred feet across along the entire length, with four lanes of traffic arranged in a two-way couplet, or set of one-way streets. Large medians placed in the middle of said lanes from Highway 246 to the junction with Highway 101 to the north.

There is heavy traffic on Highway 246 continuing straight through the intersection going east/west. However, there is little traffic along Avenue of Flags. Traffic lanes along the wide right-of-way that is Avenue of Flags are compressed towards center of the intersection, creating a conventional 4-way signal interchange, with a typical 4-phase signal sequence: left-turn priority, straight, left-turn priority, straight. Signal appears to be triggered by infra-red sensors that respond to oncoming traffic. Each corner is a large bulb-out with formal landscaping and civic accoutrements. Crosswalks are paved with dark-grey keystone pavers.

Going north: 3 lanes, left-straight-right

Going south: 3 lanes, left-straight-straight/right

Going east: 4 lanes, left-straight-straight-right

Going west: 3 lanes, left-straight-straight/right

North of Highway 246 – Median:

There are 5 separate sections of grassy median of almost equal length on Avenue of Flags between Highway 246 and the intersection with the Highway 101 off-ramp and Central Avenue to the north. They are generally seventy five feet in width, and three are comprised solely of a grassy strip and tall freeway-grade light-poles spaced about a hundred and twenty feet apart. One median to the south has been landscaped extensively to include a meandering footpath and seating areas. However, this median is segregated from the adjacent businesses by its own landscaping. The median closest to Highway 246 is landscaped with various shrubbery and vegetation, but it too does not facilitate pedestrian traffic across the avenue, acting a barrier. There is a small concrete median in front of a line of shops adjacent to Highway 246 that separates parking for those facilities from the main right-of-way, so that drivers who miss the first entrance into the “parking bay” must drive around the block to re-approach the businesses.

North of Highway 246 – Intersections:

There are four intersections on Avenue of Flags between Highway 246 and the intersection with the Highway 101 off-ramp and Central Avenue to the north.

The intersections from south to north are:

1st street and the parking lot to Anderson's Restaurant

2nd street, a two-way turn-around halfway between 2nd street and Damassa street

Damassa street to the Highway 101 northbound and southbound, and

The intersection of the Highway 101 off-ramp, Jonah Park rd., and Central ave. with Avenue of Flags.

The intermediary intersections are all perpendicular inserts that are the length of the median's width. The northernmost intersection is a complex array of streets coming and going from 5 different directions, with a small island separating traffic, and a random series of stop signs at intervals that make it hard for the user to understand how to use the intersection. Freeway traffic passes through this intersection unobstructed, and thus at full speed, which poses a threat to pedestrians and vehicular cross-traffic. There are no waiting areas inside the intersection for vehicles to pause for cross-traffic should another driver act in poor judgment.

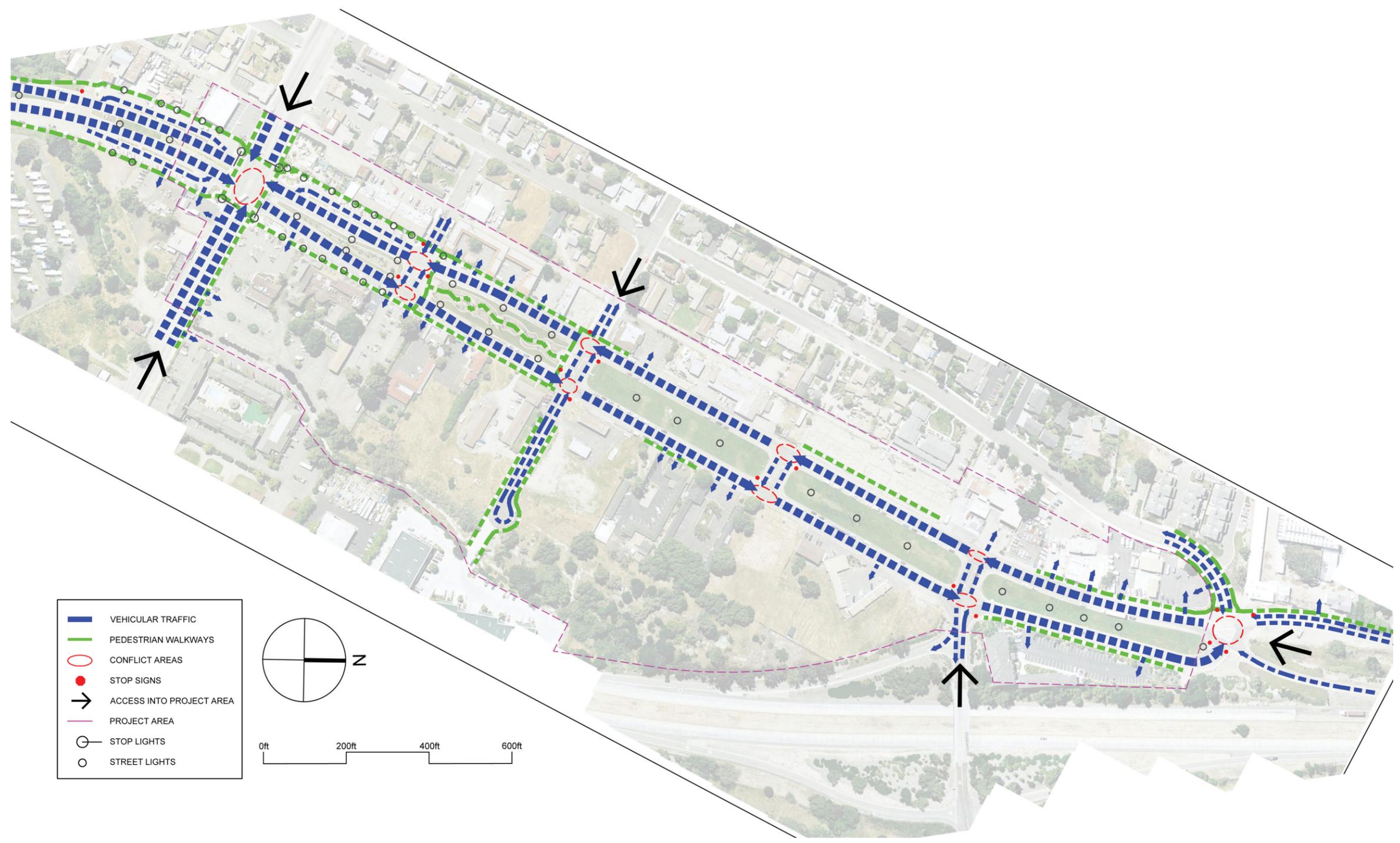
Elevation:

There is a gradual change in elevation between the southern and northern portions of Avenue of Flags of about 80", sloping northward. Along the middle of the right-of-way there is also a change in elevation of about 10" between the northbound and southbound traffic lanes, sloping westward.

Parking:

There is an abundance of parking in the area, comprised of generous on-street parking and private parking lots in almost every business along the strip. Along much of the avenue, there is parking towards the center along the central medians, being that it is to the left of the driver when driving on the one-way streets that are the avenue. I estimate there are at least 100-120 (rough estimate) parking spots in total within the project site, not including the parking lot for Patterson's Restaurant. Parking space varies in use, from private residential parking, to commercial parking, to free public parking in the street.

Circulation and Transportation Map



Road Conditions:

The street surfaces are comprised mostly of worn asphalt, but otherwise in good condition. Recent modifications have been made to the intersection of Highway 246 and the Avenue of Flags, with the placement of keystone pavers to delineate the crosswalks. There are, however, few formal sidewalks, mostly along the first two blocks of the avenue closest to Highway 246. There are long stretches of the avenue that have no sidewalk, or at the least have dirt paths—certainly not ADA accessible. What sidewalks are available are generally very narrow and constrained, at about six feet in width or less. There are a number of bulb-outs designed to reduce the width of the pavement, but these are mostly to encapsulate parking spaces. With the exception of the northern corners of the southern intersection, which have been landscaped with pedestrian usage in mind, there are no real pedestrian accoutrements. There are no less than 22 driveways along the avenue leading into various businesses.



Figure 2.5: There is a lack of formal sidewalks along the AoF. This must be dealt with before new development can be built.

2.4 Circulation and Transportation: Opportunities and Constraints

Opportunities:

Avenue of Flags/Highway 101/Jonah Park rd./Central ave.: the unique layout and geometry makes this, what is predominately a freeway off-ramp a perfect candidate for a roundabout. Roads are positioned at odd angles, and therefore already show elements of traffic diffraction due to the curving of the right-of-way's before they approach the intersection, but consistently enter and egress from five cardinal directions, forming a rounded pentagon. The main right-of-way of Highway 101 is also at a higher grade near this intersection, so a southbound on-ramp from is a viable option. Because all entry points are single lane (commensurate with the city's desire to reduce the number of lanes on the Avenue of Flags to two—one in either direction), a single lane roundabout may be sufficient to accommodate for current and future traffic flows; future traffic studies will determine this more accurately. The single lane, with the large amount of space allowed without encroaching on surrounding parcels, allows enough space for a large mountable rise in the center for trucks to safely utilize the intersection. Also, there is enough space to accommodate a second roundabout lane should traffic exceed the capacity of the roundabout.

Avenue of Flags/Highway 246: although it is a standard 4-way junction, the width of the Avenue of Flags right-of-way provides for about 200 linear feet of space to work with in one direction. Commensurate with the desires of city officials and other stakeholders, it may be feasible to build a two-lane roundabout on this point, as there is ample land to allow a radius large enough to comfortably be used by large trucks.

Vacant and Underused Land, and the Creak: there is a large amount of land on the east side of the Avenue of Flags that could be utilized to widen the street. This would allow for space in the center for a large enough median that it would become a park unto itself. A crescent road



Figure 2.6: The wide right-of-ways at the intersection of AoF and Hwy 246 allow for a variety of design solutions.

shape could be utilized to push buildings closer to the creek, which should be modified to make it a scenic landmark and an anchor for adjacent businesses.

Constraints:

Driveways: at this time, there are no less than 22 driveway cuts into private parking lots along the Avenue of Flags. These inhibit initial build options for the street, by making it harder to lay out the desired streetscape without forgoing pedestrian accoutrements.

Trucks: the Avenue of Flags is used heavily by truckers on layover to their destination. They generally stop along the Avenue of Flags, parking in the ample parallel parking spaces available. An area should be designated to concentrate their parking onto a single location.

Lack of sidewalks: currently, there is an aggregious lack of formal, paved sidewalks near the center of the project site along the Avenue of Flags. These need to be laid out before any more drastic measures are taken.



Figure 2.7: There is enough vacant and underused land to the west of AoF to build an adjacent, parallel street at marginal costs.

2.4 Natural Environment

Figure 2.8: Average Temperatures in Buellton (Source: <http://www.city-data.com/city/Buellton-California.html>)

The City of Buellton has a coastal Mediterranean climate. The summers consist of mild, dry summers that average around 80 degrees. In the winter, the climate dips down the 30's and enjoys a cool wet winter. (Figure 2.12)

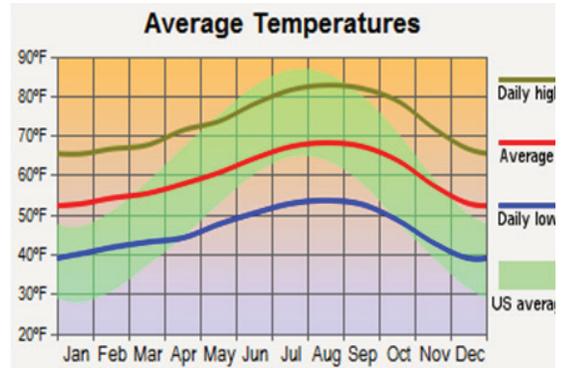


Figure 2.9: Precipitation in Buellton (Source: <http://www.city-data.com/city/Buellton-California.html>)

The City takes in about 13 inches of rain usually during the end of fall and beginning of winter. (figure 2.13).

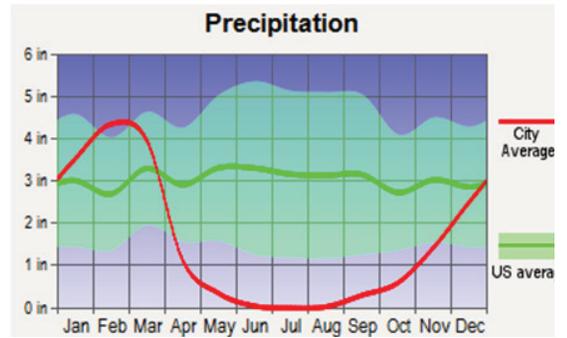


Figure 2.10 : Wind Speed in Buellton (Source: <http://www.city-data.com/city/Buellton-California.html>)

Wind Speed patterns in Buellton, California maintain a speed above 6 miles per hour. During late winter to early spring is when the wind speed starts to pick up and reach over 8 miles an hour. (Figure 2.14).

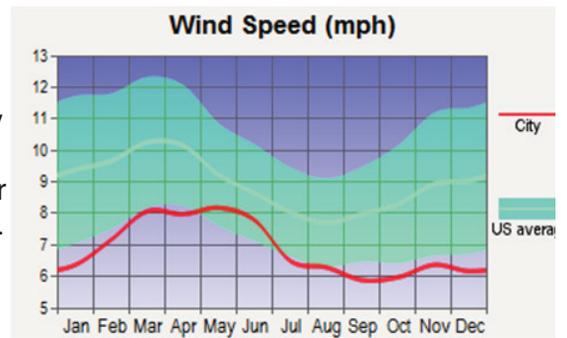
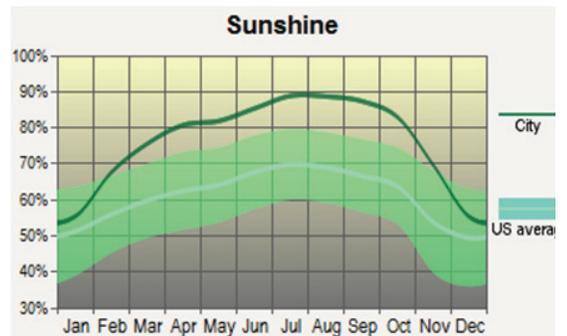


Figure 2.11 : Sunshine in Buellton (Source: <http://www.city-data.com/city/Buellton-California.html>)

Sunshine in Buellton is very prominent. It never drops below 60% and in the summer it would get as high as 90% (Figure 2.15)



Buellton has different native and existing vegetation due to the natural resources that surround the Buellton area. The area contains many varieties of open space, fertile soils, creeks and clean air. The Santa Ynez River and the Zaca Creek play a vital role in the high variety of plants and animals. There are a variety of Oak trees that surround the boundaries of Buellton. The Santa Ynez River also contains a wetland habitat which is essential for different types of species.

There are large sources of noise that surround the City of Buellton. Highway 101 cuts through the middle of Buellton, this causes a constant amount of noise to the city. The city also has Highway 246 running through the middle of the city as well. Even though this road do not have a high speed limit like the 101, it still brings in a constant amount of cars, which causes the street to be rather noisy.

The City is surrounded by hills and green space (See Figure 2.16). On the upper incline of the Avenue of Flags, there is a view shed directly into the larger hills in the area. There is also a river that can be seen from the site. This is a special physical attribute because this does show the different scenic shots of Buellton.



Figure 2.12: The hills and green space surround the city of Buellton

Opportunities:

The Avenue of Flags has a large elevation change from the north to the south. With that elevation change, it makes a wonderful view shed facing the south. Located to the east and the south of Avenue of Flags are Zaca Creek and San Ynez River. These sources of water bring different types of vegetation and animals. This also allows the city to have an abundant source of water. In the middle of the Avenue of Flags are large medians. Although they are not used, they do provide a large green open space for its residents and provide an opportunity for providing more public space for the future.

Constraints:

Some of the different constraints that Avenue of Flags contain is that it sits on a 500 and a 100 year flood plain. This flood plain covers half of the Avenue of Flags and a good part of the City. A different constraint is the two highways that cross at Buellton. This creates a constant amount of noise. Another constraint is the large number of unused gas stations that are or were present. Older gas stations would contaminate the land, making the land unusable.

Natural Environment Map



2.5 Relevant Documents

In 2002, an Urban Design Plan for Avenue of Flags/Highway 246 was proposed. The plan was not implemented to its maximum capabilities and in 2011 the City of Buellton and its residents developed a Buellton Vision Plan describing a new vision for the future Avenue of Flags/ Highway 246. The city also wrote Ordinance No. 11-07 to aid with the future implementation of the new vision plan. The Ordinance changed the zoning and municipal code to favor mixed-use developments and new developments along the Avenue and Highway 246.

Through a series of civic meetings and community workshops, the city of Buellton identified the main elements for the 2002 Urban Design Plan. The main foci in the Urban Design Plan were to redefine pedestrian spaces and streetscapes, create a commercial core on the Avenue of Flags, rehabilitate residential areas, and improve public facilities along the Avenue and Highway 246. The main objectives in improving the streetscapes were to unify vehicular signage, increase the pedestrian use of the medians, and to create a safer, more attractive pedestrian experience along the avenue. The plan aimed to widen sidewalks along the Avenue, calm traffic, and use textured pavers to indicate pedestrian accessibility. The plan also proposed street furniture and landscaping along the sidewalks and the medians. In addition, converting the medians into a linear park was proposed. In order to develop a central core, downtown, the plan proposed lining the Avenue of Flags with mixed-use buildings for retail, commercial, office, and residential uses.

The 2002 Urban Design Plan's 'Implementation Process' updated the General Plan. In this update, the General Plan was revised to include new urban design elements and policies in support of the 2002 Urban Design Plan for redesigning the Avenue of Flag/Highway 246. Buellton also updated their zoning ordinance to create more flexible zoning regulations for the city. Height limits, lot coverage, setbacks and other standards on and around the Avenue of Flags were updated to be parallel with those speculated in the 2002 Urban Design Plan. The city redefined

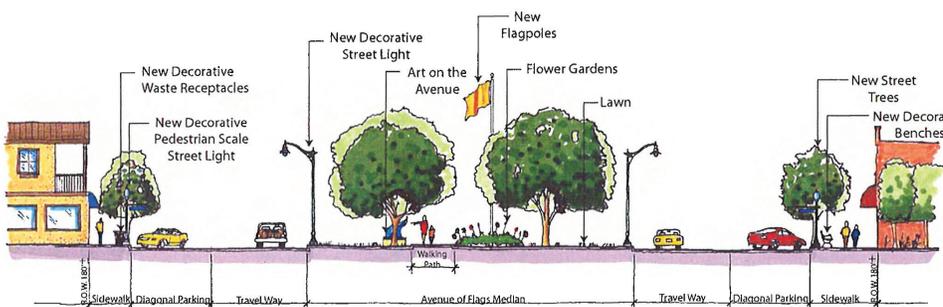


Figure 2.13: The proposed Streetscape in 2002. (Source: City of Buellton Avenue of Flags/ Highway 246 Urban Design Plan)

Buellton's Urban Design Guidelines to be more specific and to place emphasis on the design areas vital to the redevelopment of the Avenue of Flags.

Through 10 months of visioning surveys and community workshops, key ideas for a future community were drafted in the 2011 Buellton Vision Plan. The Buellton Vision Plan shares similarities with the 2002 Urban Design Plan, however it is more specific in the aspirations of the community members for the future of the Avenue of Flags. The core foci for the vision plan are similar in that they propose enhancing the pedestrian aspects, creating more attractive and accessible streetscapes, and developing a central downtown core along the avenue. However, there are differences in the 2011 vision as well. The 2011 vision explains the importance of creating a community-centered downtown driven by local business and welcoming surroundings.

The vision explains the importance of creating a vibrant downtown which brands Buellton as a unique destination hub for the the Santa Ynez Valley. This idea of Buellton as a hub is a new idea, which evolves Buellton out of its history as a stop-over, service town. The vision for the future Avenue of Flags emphasizes active, healthy living, characterized by a quaint, walkable downtown. The Vision Plan proposes to emphasize healthy living through providing a walkable downtown with options for recreation and healthy food sources, such as a Farmer's Market. The plan proposes that the downtown be a cultural center. Accordingly, it should be accessible for events, night-life, parades and festivals, and it should feature local artists. The community is not focused on maintaining the current look or feel of the Avenue of Flags. On the contrary, the vision proposes to stray from the current wide, linear street structure. The plan encourages the Avenue to be redesigned so that it is either narrowed, curvilinear, re-routed to one side of the medians, or some combination of the three options. The community would like to include a large, central plaza for events and to make the Avenue more welcoming for pedestrians. Whatever option is chosen, the vision states that the future of the Avenue of Flags must be a walkable, lively, downtown core.



Figure 2.14: Buellton's 2011 Vision plan of a Vibrant Downtown. (Source: Buellton Vision Plan)

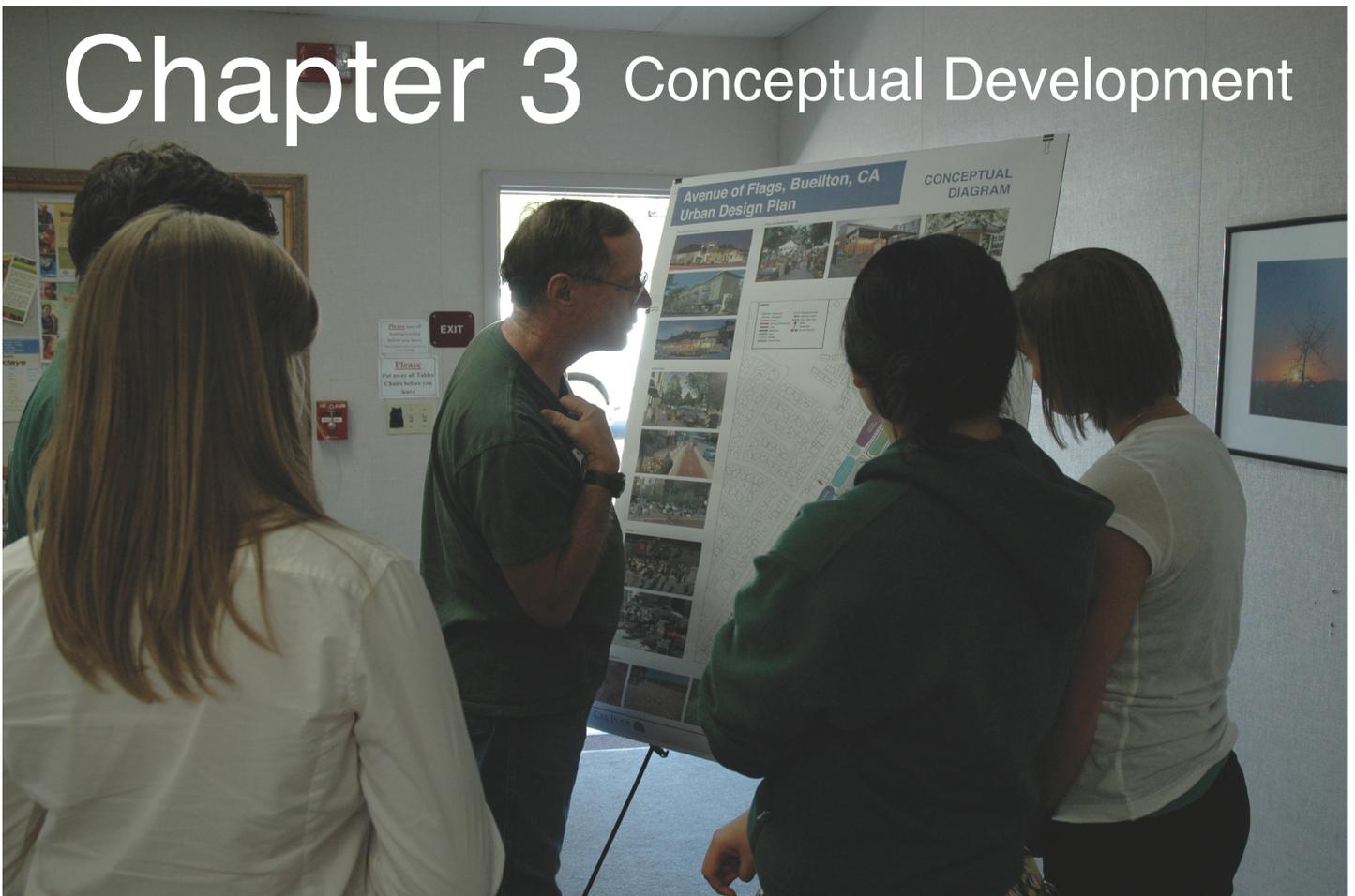
Figure 2.15: Buellton Proposed Land Use Map in 2002. (Source: City of Buellton Avenue of Flags/ Highway 246 Urban Design Plan)



Proposed Land Use Map



Chapter 3 Conceptual Development



3.1 Introduction

This chapter introduces the conceptual development process. The first section introduces the Ideation process, which marked the start of the conceptual diagram. The second section discusses Community Meeting II and the information the community brought to the team's attention. The third section presents the changes made to the conceptual diagram in regard to the findings from the community meeting.



Figure 3.1: Students interacting with the community members during Community Meeting II

3.2 Ideation

After having read the Buellton Vision Plan and the 2002 Buellton Urban Design Plan they read the Avenue of Flags Corridor Constraints Study, which became available on April 4th. These documents helped identify the most pivotal changes needed for the avenue.

The comments from the first Buellton community meeting and the information from the Buellton documents helped inform the development of the team's conceptual diagram for Avenue of Flags. The team understood that the Avenue of Flags needed to become a communal downtown center. Having a downtown center requires having a walkable, charming, gathering space for the local community where festivals, farmers markets, concerts and the like can be held. A downtown center requires good signage for businesses. It also requires that the businesses are economically viable, and they fit the community's needs. The team learned that for Buellton, a downtown center means having an entertaining destination to peruse and enjoy during the day and night.

The team proposed to create a community-gathering place on the median on Second Street after hearing from the public that this would be the most accessible median to transform into a plaza. The team proposed the plaza to extend out to the eastern business's storefront, cutting off the eastern side of the avenue to vehicular thoroughfare. This plaza would be intended for pedestrian and bicycle use only. It would also be able to host a food and drink kiosks with outdoor seating. The plaza would provide a clear view of the mountains for people to enjoy while visiting the plaza.



Figure 3.2: An idea of a farmers market would work in the paved plaza

After hearing that the community wanted Buellton to become a destination town, not a service town, the team brainstormed ideas about how to draw outsiders into Buellton to stay for the day or a long weekend. The team also heard that Buellton was a family town, but they wanted to have nightlife as well. With this information, the team decided an appropriate response to these concerns would be to propose three districts that would activate at different times of the day and also transition into one another.

The first district to activate during the day would be the Family District; it would be open earlier than the others for breakfast and brunch. It would



Figure 3.2: A paved plaza that could support community events

serve tourist and local families. Pea Soup Anderson's already brings in a family oriented clientele to the city so the team decided to expand on the idea of family in this area, making the southernmost district the Family District. North of Pea Soup Anderson's, a family recreation center with indoor and outdoor activities was proposed. Family recreation uses would attract more tourists to Buellton as a destination and provide activities for the local families to enjoy. On the West side of the avenue, brunch restaurants and a revitalized market were proposed. Retail targeted at local family and tourist's needs were also designed to be on the Western side of the avenue.

Next to the retail are proposed lunch spots, acting as a transition from morning to afternoon and leading into the next district. The intersection at Second Street and Avenue of Flags would be the Civic District. The Eastern corners of the intersection are proposed to include a civic center, and a community services center with after school care and activities for the local students and seniors. Behind the community services building, an art park is proposed as an extension of the Art Gallery to the East of



Figure 3.3: Large, walkable sidewalks is proposed for the Avenue of Flags

the site. The park would provide a unique outdoor afternoon space for the civic community. On the Western corners of the intersection, restaurants open for lunch and a library annex with reference center for the students to utilize, and gallery are proposed. Office spaces are intended to be included on the second stories of the Civic District buildings. The Civic District would be a 9-5 district catering to needs of the city functions, and the needs of the community's students, seniors, and artists. At the end the Civic District and surrounding the Plaza, the late afternoon activities and nightlife begins.

The Noon to Midnight District begins with a proposed brewery as an extension to the brewery across the freeway; it is proposed to be to



Figure 3.4: An Art park is proposed next to the Art Galler which is east of the site

the North of the Library annex, facing the plaza. Outdoor seating for the restaurants would be provided on the plaza as well. This district would encompass lively entertainment such as bars and local, late-serving restaurants. This would be the district where more mixed uses are proposed. Including a second story urban hotel with restaurants below, located on the Eastern side of the Avenue, across from the Plaza. Adjacent to the hotel would be a theater. Across from the theater, on the median to the north of the Plaza, a food kiosk with indoor and outdoor seating and afternoon and night food options is proposed. This median would have seating with a view of the Southern mountain range for afternoon eating and heat lamps with pleasant lighting for nighttime eating and drinking. On the Eastern street front, a natural, healthy foods market and restaurant is proposed because there was concern about the availability of healthy food in Buellton discussed in the Vision Plan. North of the health foods location would be a car gallery to show off the vehicular historical ties of Buellton.

This gallery would transition into the truck area of the avenue. The team recognized that the service for truckers in Buellton is important and not to be overlooked. The community seemed to be questioning what exactly to do with this population of people. The truckers need a place to park and sleep during their long journeys and Buellton is a good place to stop, considering there is a gas station offering diesel fuel and hotels. The team decided to propose large-truck parking next to the gas station so the trucks would not have to drive down the redesigned, pedestrian friendly, Avenue of Flags. The team also proposed to keep the northernmost hotel for the truck drivers stopping off for the night. North of the gas station, on the East side of the Avenue, a 50s train-car diner is proposed. The diner ties into the transportation theme of the truck use and to act as a welcoming symbol for the city because this parcel is the first visible parcel when vehicles come into Buellton off of the freeway.

The team learned that modifying land uses for a lively and entertaining downtown center was not enough. The most important element of downtown is that it needs to be walkable. Through the use of wide sidewalks, large, visible crosswalks, and providing accessibility to all areas of the avenue by foot, the team plans to make the Avenue centered on the pedestrian experience. There would be a differentiation of vehicular, cyclist, and pedestrian paving styles to call extra attention to the different uses, making the avenue safer. Street trees for shade and beautification would be planted. Public toilets would be easily accessible along the avenue. There would also be identifiable signage for each business and for pedestrian right-of-way. The community members were not only concerned about walkability but also about creating a walking path for the residents. The team took this to heart and proposed the open space adjacent to Zaca Creek to be restored. The team proposed to create a nature walk, equipped with a clear pathway, and informational signage to be located around the creek and extend from Highway 246 to the North side of Avenue of Flags.



Figure 3.5: Community members reviewing at each teams proposed site plan

3.3 Community Meeting II Findings

This section discusses the Community Meeting II in Buellton and the findings gathered. The meeting was held at Buellton Recreation Center in a conference room. The class set up four easels for the four team conceptual diagrams. The community members went around the room looking at each conceptual diagram giving input. Six red dots and six green dots were passed out to the participants. The red dots were put on the wish poem poster next to the wishes they think are the most important. Then the green dots were put on the have poem poster next to the haves they think are the most important. The last exercise the community participated in was to create their own conceptual diagram.

The input received from the participants brought to the team’s attention several things. First, there needs to be more parking available on the site. Either through out the site or in concentrated areas, such as a parking structure behind Pea Soup Anderson’s. Several of the community members liked having businesses that would take advantage of the creek nature preserve. Overall the participants liked the three districts the team created. One other point that was taken into consideration is adding signage for people to know what is on either side of the street.

The community has a number of features that they like about the city:

The creek

The good location

Proximity to the San Ynez River

Buellton is considered the Hub of the Valley

The community also had a number of features that they wish the city had:

A pedestrian friendly avenue

Unique architectural elements

A creek connection and natural hiking

A unique restaurant that could serve people after 5 pm

The conceptual diagrams the community members created have helped influence all of the teams. One conceptual diagram brought to everyone’s attention that the teams need to think about the design in phases and how everything will pan out. Another that was brought to the team’s attention is that the median strongly divides the Avenue.

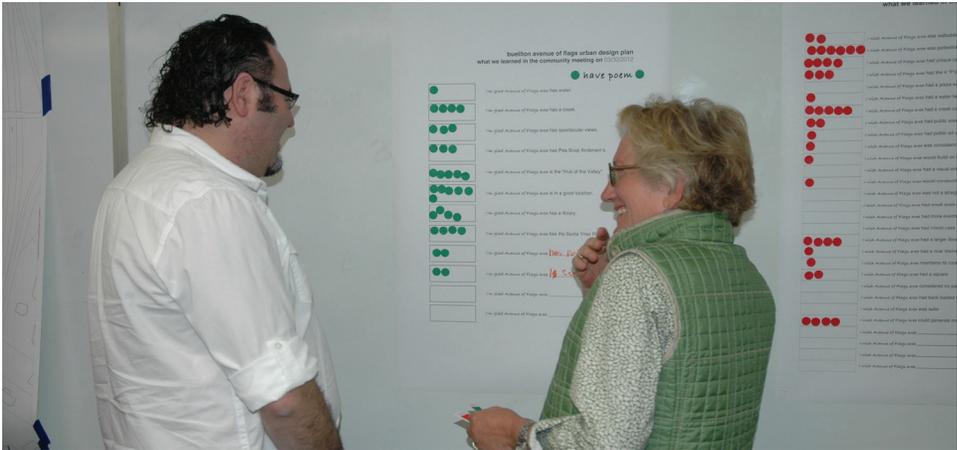


Figure 3.6: Umut interacting with the Community members

3.4 Team Conceptual Plan

The community members had a positive response to the team conceptual diagram and urban design concepts. They liked the fact that the team proposed converting the medians into plaza's and meeting kiosks. They liked the fact that the team had blocked traffic in one of the medians to turn into a large plaza. There were a few recommendations. The community members noticed that there was a lack of parking on the conceptual diagram. The team had proposed two parking structures, but they also planned to put in a few parking spaced throughout the site. The community members also recommended moving the transit hub next to the truck parking. This would be easier to access than if it was in the dense part of the plaza.

The team proposed the three different districts because this would be essential to the plaza. Having a large plaza and civic area, would attract many people into its plaza. The family center would attract those who are going to Anderson's Pea Soup to stop and have a place where the children could have fun. The night life is directed to the Santa Ynez Valley. The team proposed a place where people could go to past 5 pm.

3.5 Concept Diagram





Chapter 4 Land Use and Circulation

4.1 Introduction

For this phase of the project, the team developed land use, circulation, and the public space decisions. Land use decisions determine what the use of the structure would be. The team wanted to incorporate the values and the importance of the planned public space that Buellton needs. The team went through Community Meetings III and IV and found what the community liked in the conceptual plans and the illustrative site plan. The team proposed regulations and the reasons as to the rationale behind the land uses. The team has also begun developing circulation propositions with the way the parcels and land uses were proposed. Finally, the team has proposed several different community amenities that will help the community develop a downtown and a gathering space.

4.2 Summary of Findings

On April 30th, 2012, a meeting between the community and the team was held at the Buellton Recreation Center. The team members discussed the revised land use map and illustrative site plan with the community. The team placed the land use map and illustrative site plan posters on easels and allowed the community to draw, write, and place 'like' and 'dislike' stickers on elements of the posters.



Figure 4.1 Buellton Recreation Center

The team heard both critiques and praise from the community during the meeting. Some of the critiques were about parking. The community voiced that the designated parking areas needed to be more apparent and that there should be more parking spaces provided. Some people said the main plaza and civic corners along Second Street created too much space and that they needed to be scaled back.

In general, the community liked the revisions made by the team. They liked the removed medians, South of Second Street on the Avenue of

Flags, which allowed for more parcels to line the avenue, creating a greater commercial street presence. The community also liked the nature walk proposed in the open space, connecting Highway 246 to Highway 101 and Avenue of Flags. The community liked this proposal because it enables a loop of pedestrian access from Zaca Creek and through downtown. They also had positive reactions to the proposed easements through parcels that would link the nature path on the west side of Zaca Creek to the Avenue of Flags.

The team heard some conflicting messages from the community as well. While some community members liked the proposed train-car diner to the North of the site for its welcoming image when drivers exit off of Highway 101 into Buellton, others felt the diner was misguided and in “no-man’s land.” There were also mixed opinions about having a designated truck parking North –West of the Avenue. Some thought that the truck parking was necessary and that it was a good idea to place it adjacent to the existing gas station and across from the existing hotel, whereas other people were worried that truck parking would be too noisy for the



Figure 4.2 Community members around the team’s land use and illustrative site plan posters

residents who live North-West of the site. Some people also liked the proposed amphitheater in the main plaza and others liked it but thought it should be placed next to the creek, in the open space to mitigate against the possible noise it could create.

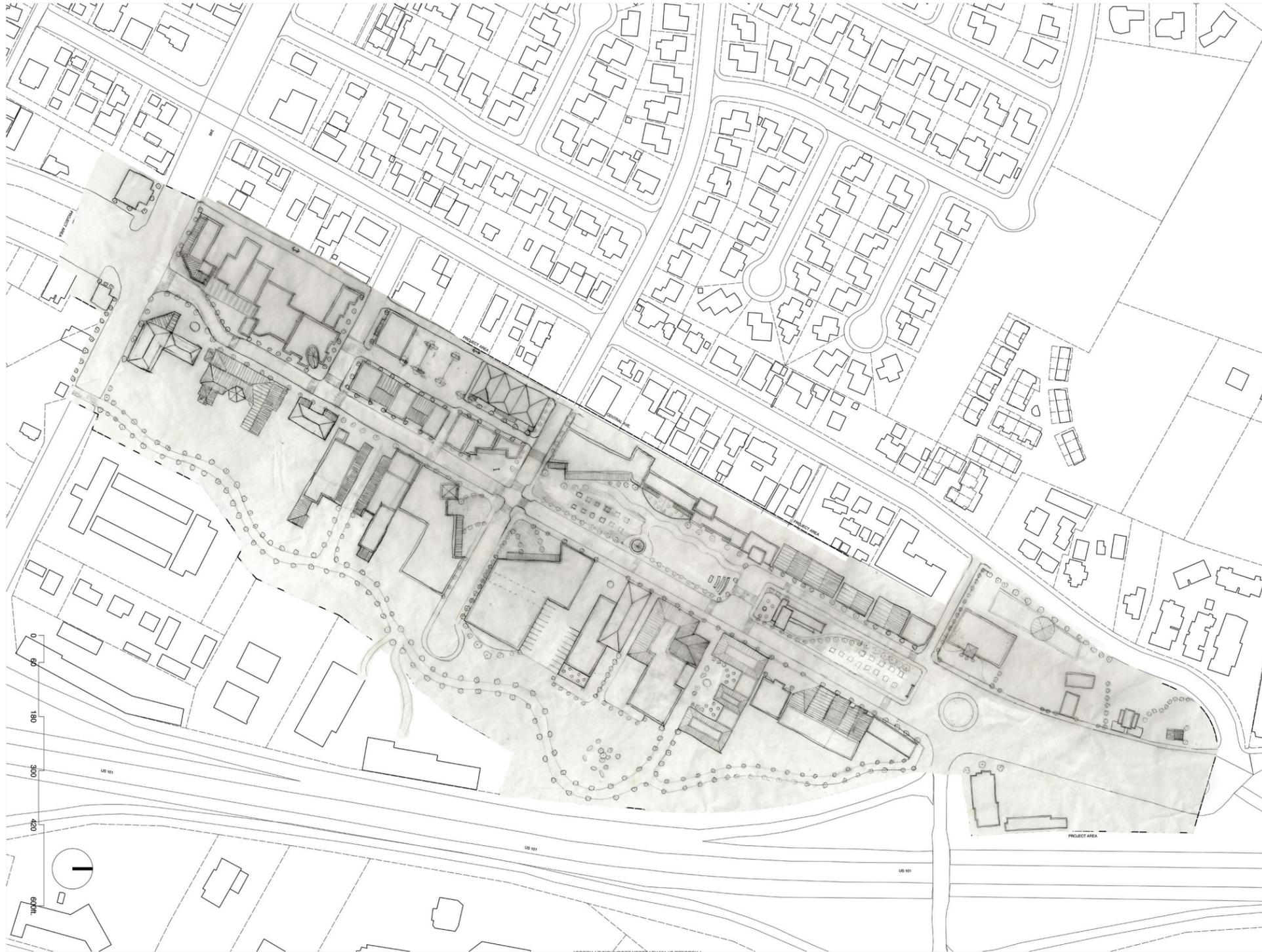
4.3 Proposed Land Uses

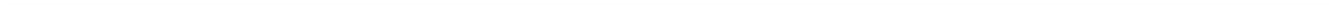
The proposed land uses were similar to the first land use map, however some land uses were reorganized and the shape and size of the Avenue and some parcels changed. On the south side of the Avenue of Flags, where it intersects Highway 246, the land uses are still proposed as commercial uses. The uses were not revised because the community response from the first meeting was generally positive about this area.

4.4 Regulating Land Use Map



4.5 Illustrative Site Plan





South of Second Street, along the west side of the Avenue, all of the uses are dedicated to family oriented commercial development. Examples of this type of development would be breakfast/brunch restaurants, toy stores, gift shops, ice cream parlors and the like. Across from these proposed family uses are Pea Soup Andersons, which is intended to stay and North of Pea Soup, a family recreation center is proposed.



Figure 4.3 Community members around the team's illustrative site plan poster

At the intersection of the Avenue of Flags and Second Street, a civic district with a community services building dedicated to after-school programs and adult education is proposed. The civic district is intended to have a reference center, gallery, and community meeting rooms. On the second floor of some of these buildings, office space is proposed. The civic center would also have quick lunch restaurants, catering to the local businesses and civic area.

North of the civic district, and south of the transit district, the 'Noon to Midnight' district remained a strong proposal because of the positive responses from past community meetings. On The East of the Avenue, North of the Civic center, mixed-use developments are proposed to complement the existing mixed-use building across the street. The mixed-use is intended to have retail and commercial on the first floor and a hotel on the second floor. There will also be parking for the hotel and commercial in the rear of the building, behind the commercial and under the hotel. A similar mixed-use building is proposed adjacent to the mixed-use hotel, however, instead of having a hotel on the second story, affordable housing is suggested. North of the second mixed-use building, more commercial buildings, a parking structure, and a theater are proposed. The median North of Second Street is proposed to be the central plaza, which would extend up to the storefronts on the West

side of the Avenue. The parcels adjacent to the main plaza and South of the existing mixed-use building are proposed to provide commercial uses such as restaurants with seating in front, on the plaza. North of the existing mixed-use building, on the West side of the Avenue, a Transit and Vehicular Gallery is proposed because the history of Buellton is tied to transportation.

North of the Noon to Midnight district a transit district is proposed. The gas station would remain because of the diesel it provides to the trucks that stop over in Buellton. Adjacent to the gas station a truck parking lot is proposed so the truckers could park and stay in the hotel across the street, which is intended to remain on the same parcel.

4.6 Community Amenities

This section introduces the public amenities the team has proposed for the Avenue of Flags. There are four proposed public amenities, which include three plazas and a park.

The central plaza is proposed to be at Second Street and the Avenue of Flags. The plaza is an offset square that achieves a couple objectives. First, it creates a cohesive space between the three proposed community buildings. Second, it creates a funnel into the larger plaza to the north and another funnel to the retail area at the south.

The larger plaza would span from the north end of the central plaza to Damassa Street. The residents of Buellton have mentioned to the team the need of a plaza that would serve as a gathering area. The team has, therefore, proposed a plaza that would take up the two northern medians. One median would be partially paved for different events, such as a farmers' market. Also on that transformed median would be a small grass amphitheater that would take advantage of the view shed to the south. On the median leading up to Damassa Street would be a welcome sign to Buellton and an interactive public installation. Some of the proposed land uses surrounding this plaza would be a library and an art gallery.

Figure 4.4 (left) Potential example of seating along streets or plazas



Figure 4.5 (right) Example of a nature path



The third plaza is at the south end of the Avenue of Flags and crosses over the Avenue. Several community members mentioned to the team there is need of a feature that would attract people further into the site. The placement of this plaza addresses the need of a lure from Highway 246. It also doubles as a plaza for families to use when shopping at the proposed retail services.

A prominent community amenity proposed is the park that spans from Highway 246 to Damassa Street. The park has three spaces that have unique characteristics. The features within the open space park have originated from the findings at community meetings. Residents have indicated they wanted a walking/biking trail along Zaca Creek, therefore the team has suggested a trail starting from Highway 246 to Damassa Street. As a person walks along the trail, he or she would be able to read informational signs about the history and nature that is in Buellton. The signs address the want of an interactive space and an interesting walk for families.



Figure 4.6 (left) Informational signage for a nature walk

Figure 4.7 (center) Interactive and informational totem pole

4.8 (right) Example of a climable sculpture for potential children mock zoo and play area

The team also proposed an art park behind the community center on the south side of Second Street. The purpose of the art park is to create a space for the people working in the proposed offices above the community center a space to take a break in and to display the outdoor art of local artists.

A little north of the art park would be an interactive playground. This playground would be designed to be informative, and through interactive features it would support children's learning about the local area. The playground would have different sculptures for children to climb on, such as a bear, which would create a connection back to the properties original use as a zoo.

The variety of public amenities throughout the Avenue of Flags will create a space that will support all ages and groups of people.

4.7 Circulation

Concurrent with the wishes of community members, our team proposes various circulation alterations that help to promote pedestrianism, and create the feel of a small urban space. The team's intent is to braid an urban fabric that attracts tourism and pleases the locals; to use surface accoutrements that are aesthetically pleasing and ecologically resourceful, such as porous paving and low-maintenance landscaping; and to calm traffic to the point where drivers do not feel the road belongs exclusively to them, so that pedestrians feel comfortable traversing the street without treating it like a barrier. To achieve this goal, the team proposes the following design objectives:

The Avenue of Flags will first be reduced to two lanes, with one travelling in either direction. There are a series of physical alterations to the streetscape to make it more accessible to the pedestrian, and relegate cars to a secondary element as part of the street.

Figure 4.9 Zeist Voorrangskruispunt: example of small roundabout for Avenue of Flags and Damasus



From Highway 246, the street diverts right and passes diagonally through an open plaza at the entrance to the downtown area. It then continues along its previous axis angled north-north-west. This “weave” is intended force cars to address the sharp turns at slow speeds, and therefore make it safer for pedestrians to move about freely. It would be feasible to place bollards along the street at this point, so that it is seen as an element of the plaza, not a cut-out of it, and also so pedestrians will feel comfortable crossing the street at leisure. Said plaza is arranged so as to reveal the most of the profile of Anderson’s Pea Soup restaurant, reinforcing its status as an architectural icon and landmark, however dubious a sentiment that may be.

As the street continues northward, the buildings on either side of the street will gradually close-in on it, making it narrower and creating a feeling of volume and place. Trees are placed along here to provide shade and reduce any wind-tunnel effect that might be produced by the surrounding buildings. This remains the case until just short of Second St., where the Avenue opens into a public square that is aligned towards true-north. This square is approximately two-hundred feet across, and is loosely situated with the intersection of Second St. and Avenue of Flags near its center. This intersection can be arranged in a non-traditional

fashion, perhaps utilizing a wide central median in the center that is wide enough to allow vehicles and bicycles to perform left-turns in two phases, without forcing vehicles travelling along the Avenue to come to a full and complete stop to allow the passage of cross-traffic. Weaving can be implemented to further slow traffic at this point, making it safer and more approachable by pedestrians

Immediately north of this square is the main civic plaza, which is a continuation of the first. The Avenue runs along the eastern side of said plaza as a two-lane street with no median. It continues like this until the end of the plaza, where it splits into a couplet of one-lane, one-way streets, each lane travelling on opposite sides of a new, landscaped median. They continue like this until the intersection with Central Ave and Jonah Park Rd. This couplet forms a loop around said median, and allows entry into parking lots on the current mixed-use development, and to the proposed parking garage on the other side of the street.

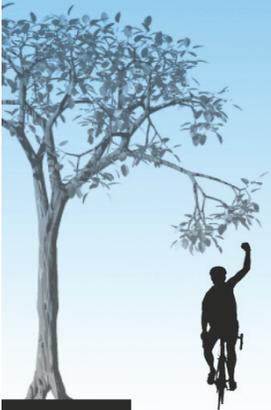
There is no parking along the Avenue. All parking will be relegated to rear parking lots behind buildings—especially along the alleyway to the west side of the site—and in externalized parking lots owned by the city. However, there may be angled parking along Second St. provided that the street is wide enough.

Generally, sidewalks will be a minimum of ten feet wide. There are a large array of pedestrian paths permeating every building; too many to mention all here. All plazas will be universally accessible to all pedestrians, and will encourage travel along the path of least resistance in all directions. One pedestrian way of note is a proposed mall that leads from the square oriented at true-north. It will go true-east until it reaches the former zoo property to the east, where it then leads north-east along the backs of the buildings that affront the zoo property. There will be businesses on either side of the mall until it reaches the zoo property, with secondary uses on the upper floors.

There is a bicycle path running all the way from Jonah Park Rd. to Highway 246, loosely following the vehicular right-of-way. At both intersections the bicycle path is intended to continue further in either direction, concurrent with the city's desire to further invest in high-quality bicycle infrastructure. Methods will be employed to ensure that the network is expandable beyond its current proposed facilities.

Using the easement already owned by the city along the eastern portion of the site, this team proposes utilizing that easement as a pedestrian walk, with connections to Highway 246 to the south, and to the intersection with Central Ave. to the north. It is intended for this path to be expandable beyond its current proposed facilities.

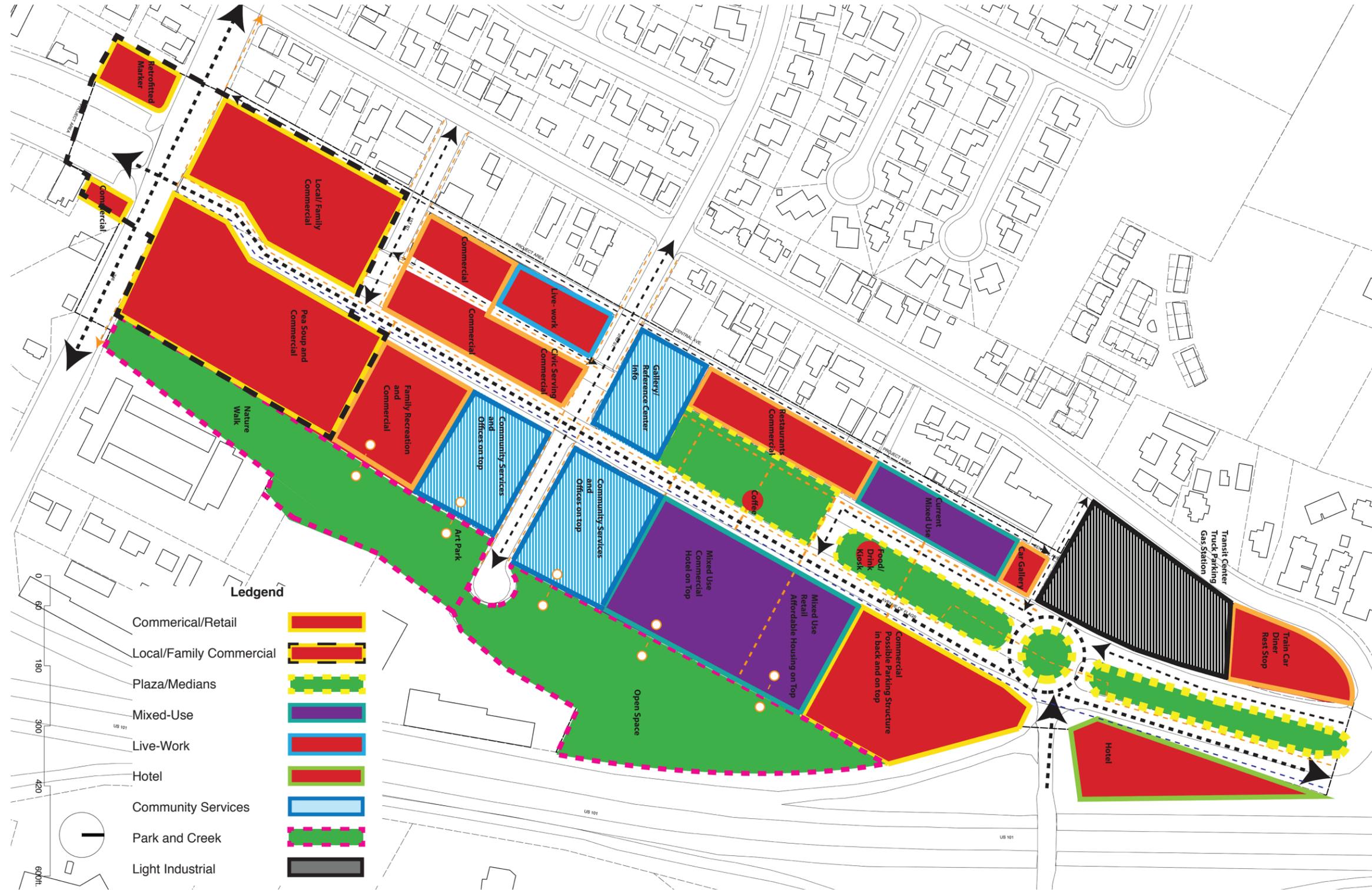
Chapter 5 Form Based Codes



5.1 Introduction

This chapter will introduce the team’s Regulating Plan, Phasing Plan, Form-Based Codes, Illustrative Site Plan, and 3D still renderings. The Regulating Plan shows the nine regulating areas with the allowed land uses. The regulating areas were chosen according to what proposed uses would fit into one set of Form-Based Codes. The Phasing Strategy discusses a three-phase process to give guidance for implementation of the team’s urban design ideas. The Form-Based Codes provide detailed guidance for future development along the Avenue of Flags. The information provided within the Form-Based Codes include setbacks and build-to-lines, fenestration, parking requirements, and allowed signage for the private parcels. Next are the streetscaping and intersections that cover the layout of the traffic lanes, location of trees and street lighting, and traffic calming measures. Then for public and private properties there are recommended low-impact development strategies. Lastly in this chapter is the team’s Illustrative Site Plan with accompanying 3D still renderings.

5.2 Regulating Plan



Ledgend

Commerical/Retail	
Local/Family Commercial	
Plaza/Medians	
Mixed-Use	
Live-Work	
Hotel	
Community Services	
Park and Creek	
Light Industrial	

5.3 Phasing Strategy

Phase 1, 0-5 years:

Streetscape Improvements:

- Elimination of parking on the Avenue
- Re-stripping of the Avenue to include bicycle lanes, and to divert vehicular traffic onto eastern lanes
- Re-configure for two-way traffic
- Plant trees along Avenue at strategic locations
- Act upon easement through Zoo property, and lay foot-path from Anderson's property to Damassa Road

Private Properties:

- Encourage property owners to stripe parking spaces along alleyway
- Acquire parking lot behind Anderson's for public purposes
- Development of parcels, and construction of civic buildings around plaza at Avenue and 2nd
- Construction of food kiosk on northern median

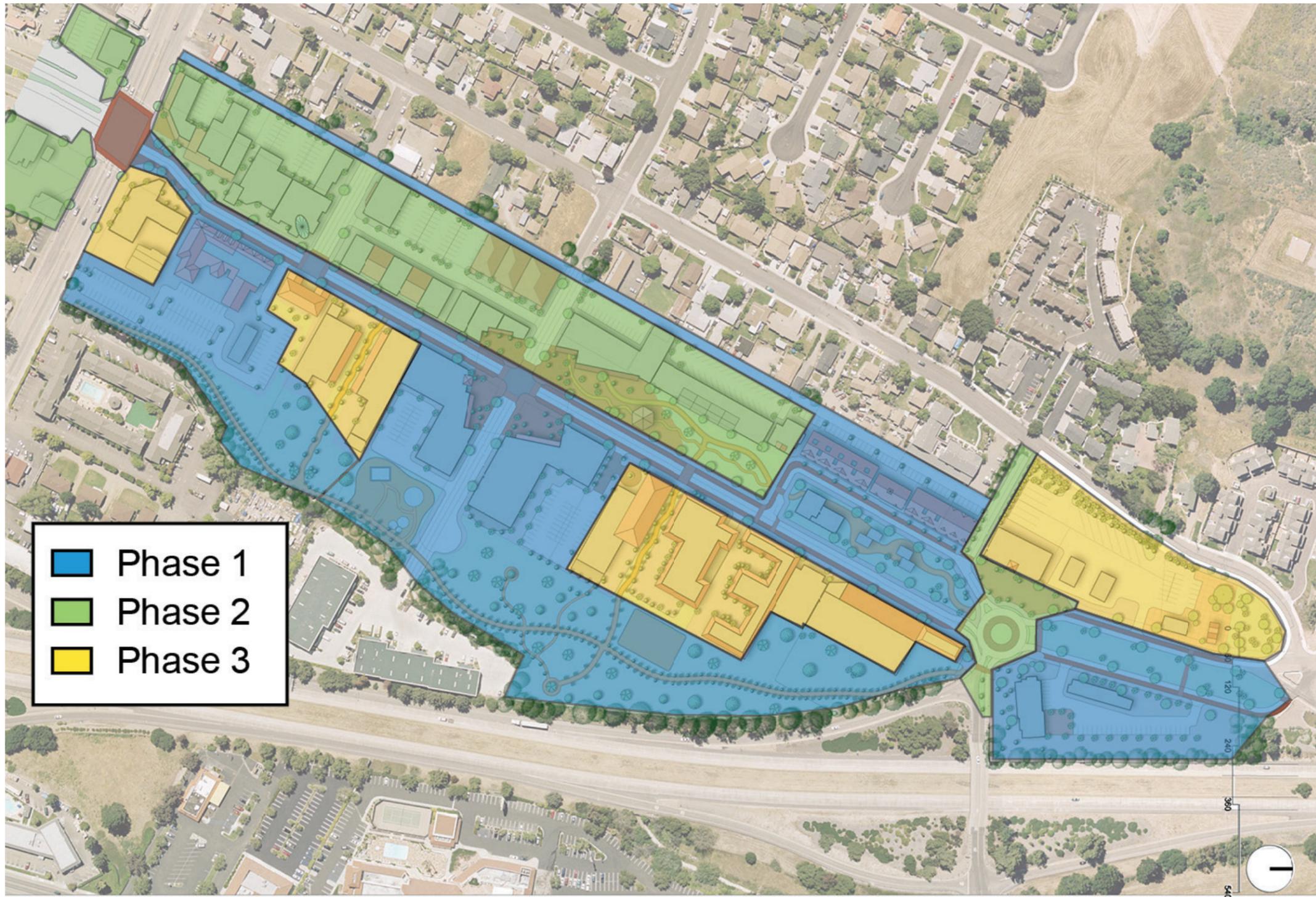
Phase 2, 5-15 years:

- Beginning of Larger Developments
- Roundabout at Damassa Rd
- Acquire median space and develop Central Square
- Relocate traffic lanes to eastern side of ROW from 2nd St. to Hwy 246
- Sell-off median land from 2nd to Hwy 246 to private developers

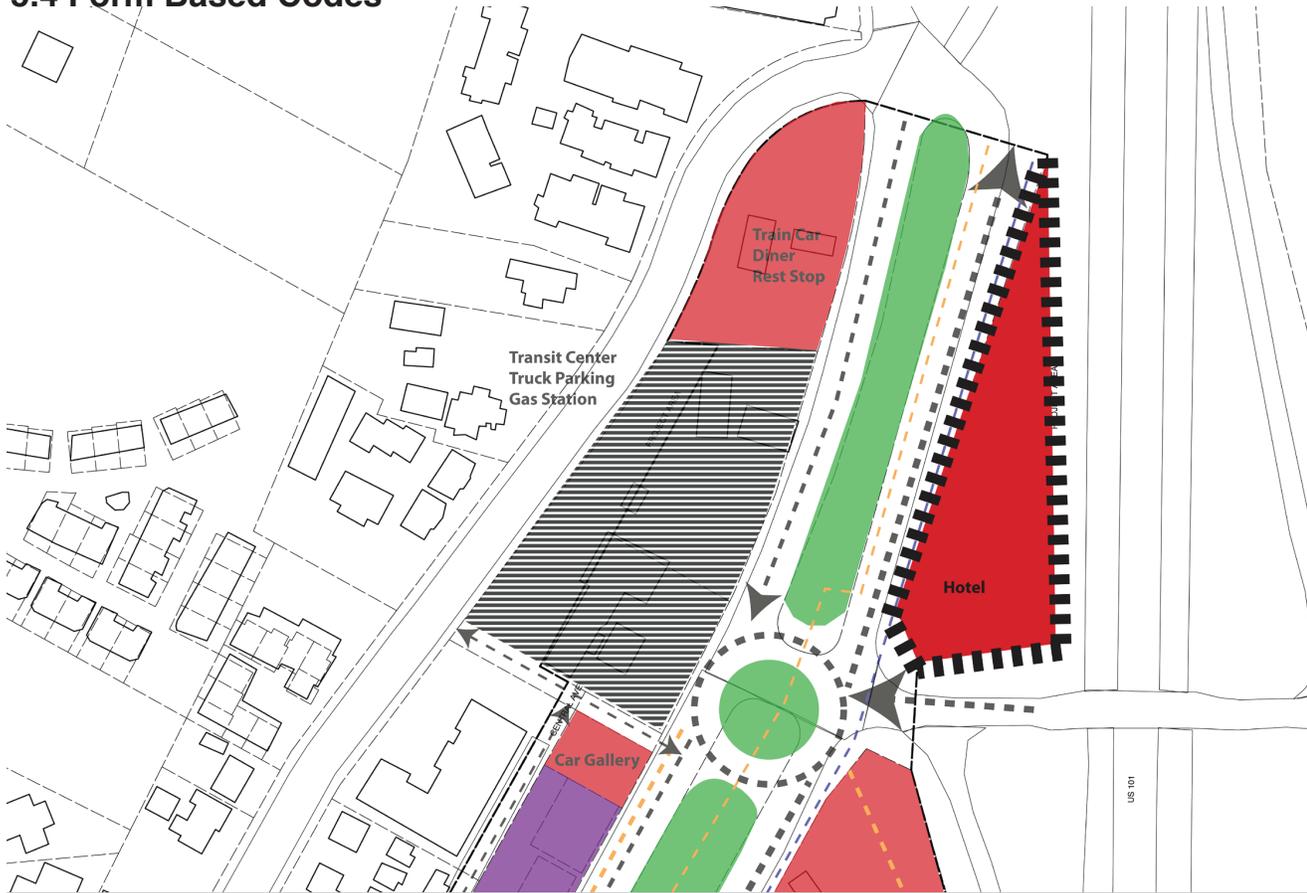
Phase 3, 15-30 years:

- Full build-out of design proposal
- Develop properties south of 2nd St.
- Relocate market at western corner of intersection of Hwy 246 and the Avenue

Phasing Plan



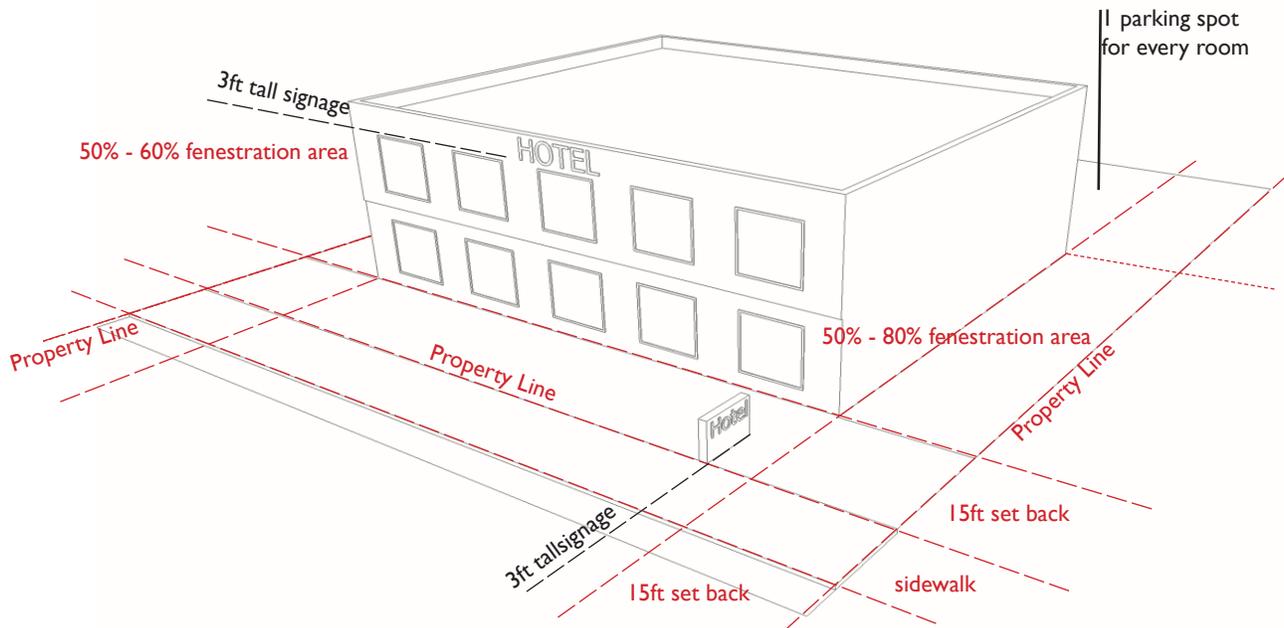
5.4 Form Based Codes



Hotel

Urban Design Objectives for the Regulation Area:

The Objective is to generate a welcoming entry to the Avenue. In order to achieve this, building(s) in this area need to be close to the Avenue, pushing parking to the back of the parcel.



Hotel I Building Siting and Massing

ALLOWED LAND USES

Ground floor	Hotel
Upper floor(s)	Hotel

SETBACKS

From street front on ground floor	5 feet min - 15 feet max
From street front on upper floor(s)	5 feet min- 15 feet max
From side property lines on ground floor	15 feet minimum
From side property lines on upper floor(s)	15 feet minimum
From back property line on ground floor	20 feet minimum for Parking
From back property line on upper floor(s)	0 feet minimum

BUILD-TO-LINES

On street front on ground floor	None
On street front on upper floor(s)	None
On side property lines on ground floor	None
On side property lines on upper floor(s)	None
On back property line on ground floor	None
On back property line on upper floor(s)	None

BUILDING HEIGHTS

Ground floor (minimum and maximum)	10 feet - 12 feet
Upper floor(s) (minimum and maximum)	10 feet - 12 feet

FENESTRATION

Required fenestration area (% total street facade) on ground floor	50% - 80%
Required fenestration area (% total street facade) on upper floor(s)	50% - 60%

PARKING REQUIREMENTS

Number of required parking spots per room of hotel	1 parking spot per room
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SIGNAGE

Allowed signage types	On facade and street front
Allowed signage heights	3-5 feet and 3-5 feet

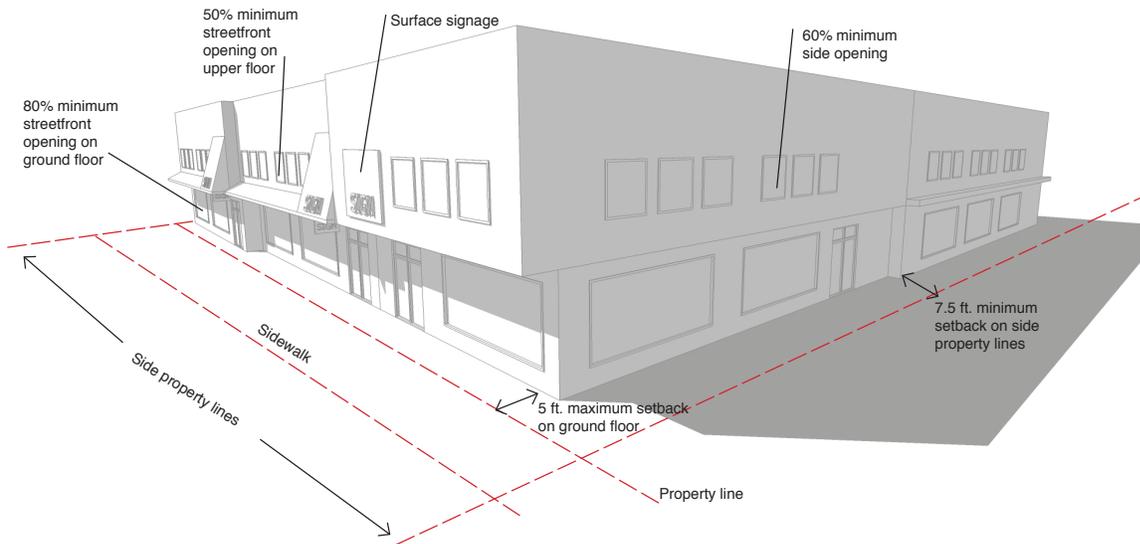


Mixed-Use: HOTEL/RESIDENTIAL OVER COMMERCIAL

Urban Design Objectives for the Regulation Area:

The mixed-use are located next to the proposed Community Center on the east side of the AoF. At the meetings in Buellton, the residents have emphasized the need for more mixed-use properties along the Avenue and to highlight Zaca Creek.

The team's first urban design objective for the mixed-use area is to create a connection with the park behind the properties. This can be achieved by creating walkways between the buildings leading towards the park and creek. The team has also proposed to have the parking towards the back of the property and would be hidden and/or covered, so it does not detract from the park. Within each individual property a inner courtyard is encouraged.



Mixed-Use (Hotel/Residential) | Building Siting and Massing

ALLOWED LAND USES	
Ground floor	Commercial
Upper floor(s)	Hotel and/or Residential
SETBACKS	
From street front on ground floor	5 ft. maximum
From street front on upper floor(s)	0 ft. maximum
From side property lines on ground floor	7.5 ft. minimum
From side property lines on upper floor(s)	7.5 ft. minimum
From back property line on ground floor	10 ft. required setback, with a minimum of 30% of facade on back property line
From back property line on upper floor(s)	Minimum 30% of facade on back property line
BUILD-TO-LINES	
On street front on ground floor	None
On street front on upper floor(s)	Street front property line = build-to-line
On side property lines on ground floor	40% of facades should meet at 7.5 ft. from property lines
On side property lines on upper floor(s)	40% of facades should meet at 7.5 ft. from property lines
On back property line on ground floor	None
On back property line on upper floor(s)	None
BUILDING HEIGHTS	
Ground floor (minimum and maximum)	12 ft. maximum
Upper floor(s) (minimum and maximum)	12 ft. maximum
FENESTRATION	
Required fenestration area (% total street facade) on ground floor	80% of total ground floor; 60% minimum on side
Required fenestration area (% total street facade) on upper floor(s)	50% of total upper floor
PARKING REQUIREMENTS	
Number of required parking spots per sq. ft. of Commercial	1 spot / 300 sqft commercial
Number of required parking spots per sq. ft. of Residential	1 spot / first BR, 1/2 spots / each additional BR
Number of required parking spots per sq. ft. of Hotel	1 spot / room
SIGNAGE	
Allowed signage types	Surface, projecting or hanging cantilever
Allowed signage heights	Between 8 - 12 ft.



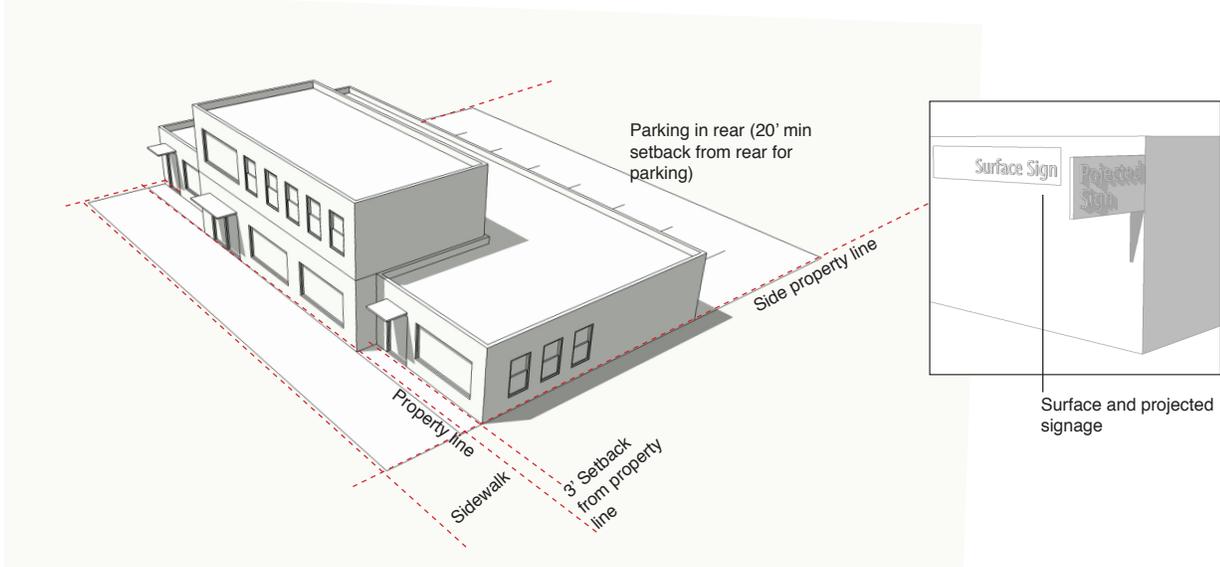
Commercial/Retail (Restaurants - Cultural)

Urban Design Objectives for the Regulation Area:

The buildings for stand-alone commercial/retail, restaurants, and cultural uses are intended to support the business of the residents of Buellton and those passing through, staying at one of the hotels on the Avenue of Flags. These uses are meant to draw the attention of locals and travelers alike, to bring them in for a day or a weekend of shopping, viewing galleries, and eating.

The objective of these areas are to :

- Provide a strong and pleasant streetfront
- Provide community and cultural activities for the day and night
- Satisfy visitors of all age groups



Commercial/Retail I Building Siting and Massing

ALLOWED LAND USES

Ground floor	Commercial/Retail - Restaurant - Cultural
Upper floor(s)	None or Secondary uses supporting ground floor commercial, such as office space or storage

SETBACKS

From street front on ground floor	3' maximum
From street front on upper floor(s)	none
From side property lines on ground floor	none, except to provide a passageway from the Zaca Creek Open Space to the Avenue of Flags, in which case a 10-15' setback on one side is required
From side property lines on upper floor(s)	none
From back property line on ground floor	20' minimum to accomodate parking
From back property line on upper floor(s)	none

BUILD-TO-LINES

On street front on ground floor	0-3' from property line=build to line, 50% of facade should be built on property line
On street front on upper floor(s)	0-3' from property line=build to line
On side property lines on ground floor	side property lines=build to lines
On side property lines on upper floor(s)	side property lines=build to lines
On back property line on ground floor	none
On back property line on upper floor(s)	none

BUILDING HEIGHTS

Ground floor (minimum and maximum)	10' minimum, 12' maximum
Upper floor(s) (minimum and maximum)	9' minimum, 11' maximum

FENESTRATION

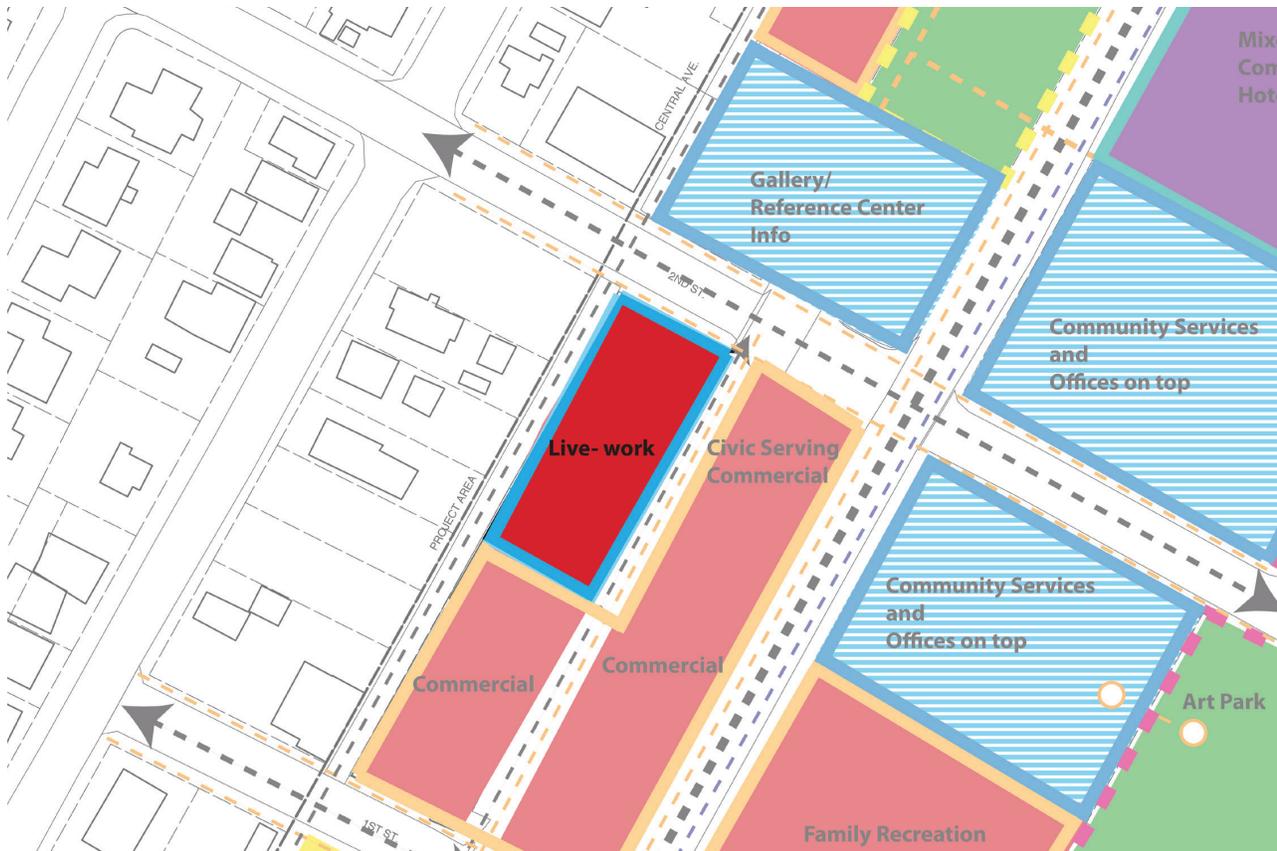
Required fenestration area (% total street facade) on ground floor	80% of total ground floor facade area
Required fenestration area (% total street facade) on upper floor(s)	50% of total ground floor facade area

PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of USE 1 (Commercial/retail - restaurant - cultural)	1 spot / 300 sqft commercial
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SIGNAGE

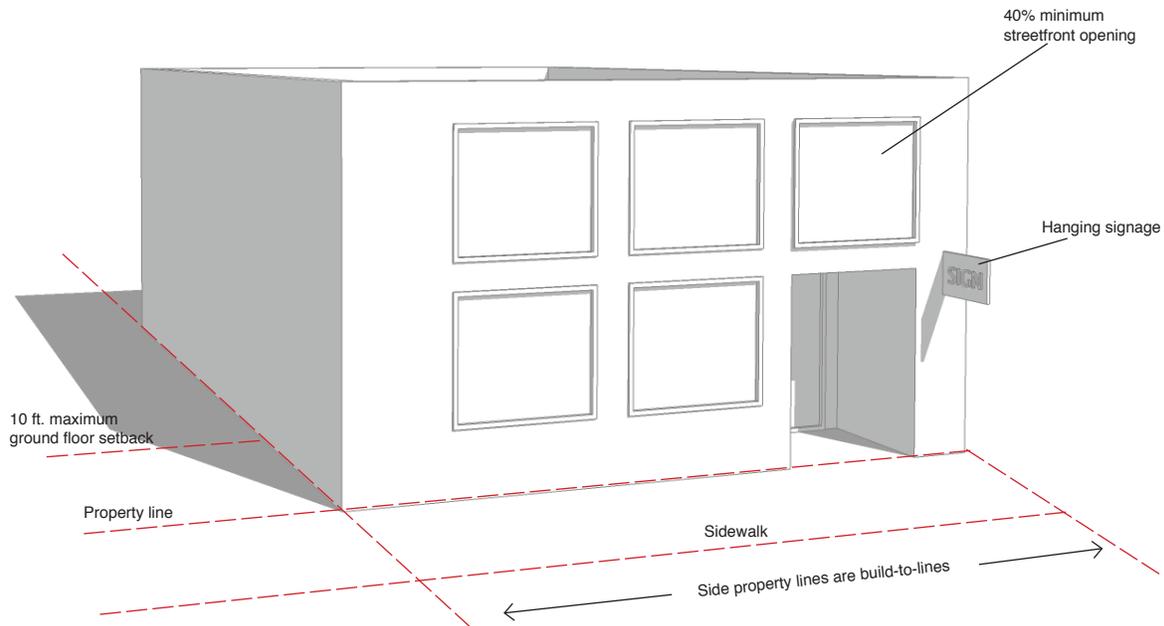
Allowed signage types	Surface and projecting/hanging required
Allowed signage heights	Between 8-12'



Live-Work

Urban Design Objectives for the Regulation Area:

The live-work regulation area is intended for businesses that do not need street frontage. The type of businesses that would fall into this category would be art studios, fitness studios, and doctor offices. Residents in Buellton have expressed the need for housing that would cater to those who want to have a live-work style. The team also intended to create a transition between the Avenue of Flags and the residential area to the west.



Live-Work I Building Siting and Massing

ALLOWED LAND USES

Ground floor Live-Work

SETBACKS

From street front on ground floor	10 ft. maximum
From street front on upper floor(s)	0 ft.
From side property lines on ground floor	None
From side property lines on upper floor(s)	None
From back property line on ground floor	20 ft. to accommodate parking
From back property line on upper floor(s)	0 - 10 ft.

BUILD-TO-LINES

On street front on ground floor	75% of facade should be built on street front property line
On street front on upper floor(s)	Street front property line = build-to-line
On side property lines on ground floor	Side property line = build-to-line
On side property lines on upper floor(s)	Side property line = build-to-line
On back property line on ground floor	None
On back property line on upper floor(s)	None

BUILDING HEIGHTS

Ground floor (minimum and maximum) 12 ft. minimum, 16 ft. maximum to allow for mezzanine (floor-to-ceiling)

FENESTRATION

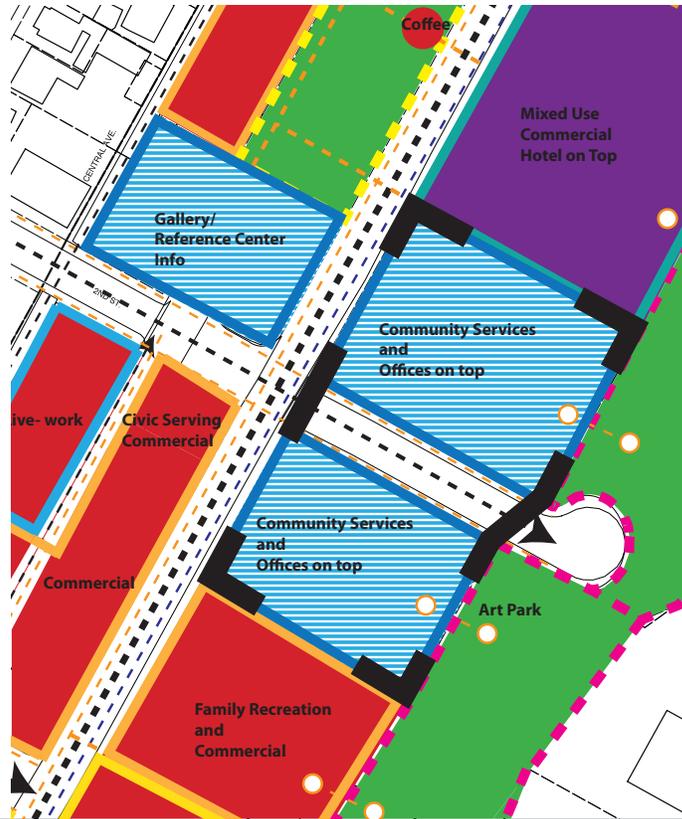
Required fenestration area (% total street facade) on ground floor	40% of total ground floor area
Required fenestration area (% total street facade) on upper floor(s)	None

PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of USE 1 (specify) 1 per unit

SIGNAGE

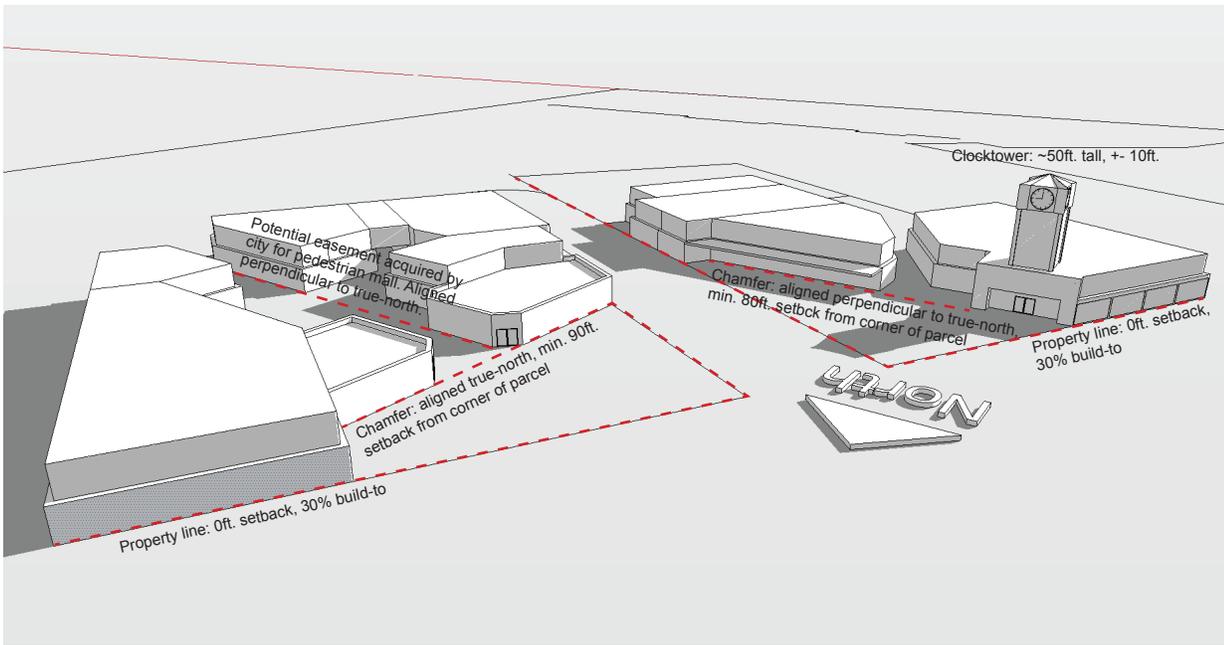
Allowed signage types	Projecting or hanging cantilever
Allowed signage heights	Between 7 - 9 ft.



Community Services and Offices

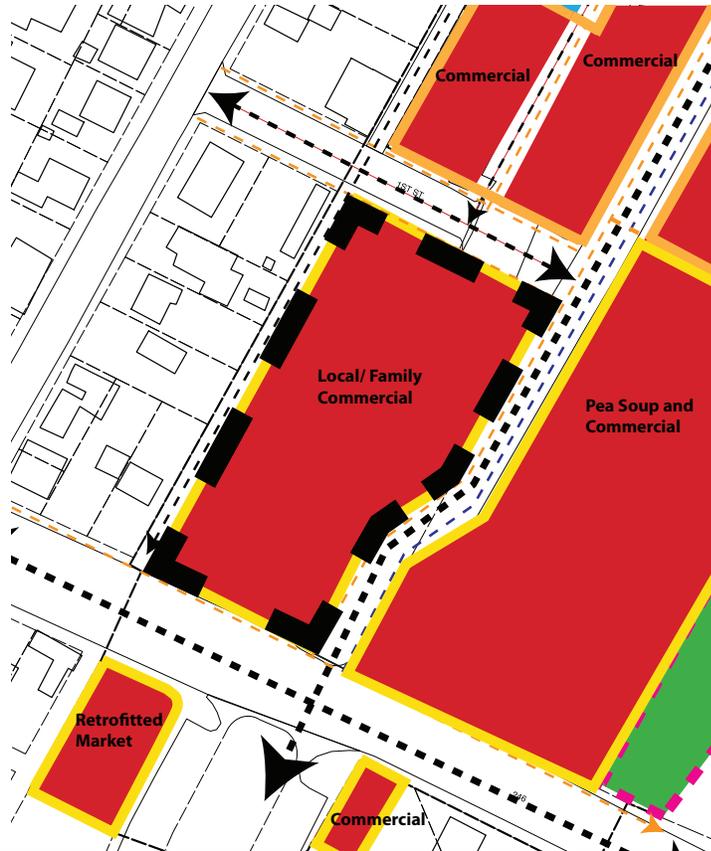
Urban Design Objectives for the Regulation Area:

To provide a Civic area that can be a spot for community gathering and general activity, as well as a major source of employment. Food and Retail services, in combination with a vibrant and well-built plaza, is proposed to spur economic activity and restore vitality to Buellton’s Core. Office employment would facilitate the long-term tenants required to ensure continued investment in the Downtown area.



Community Services and Offices

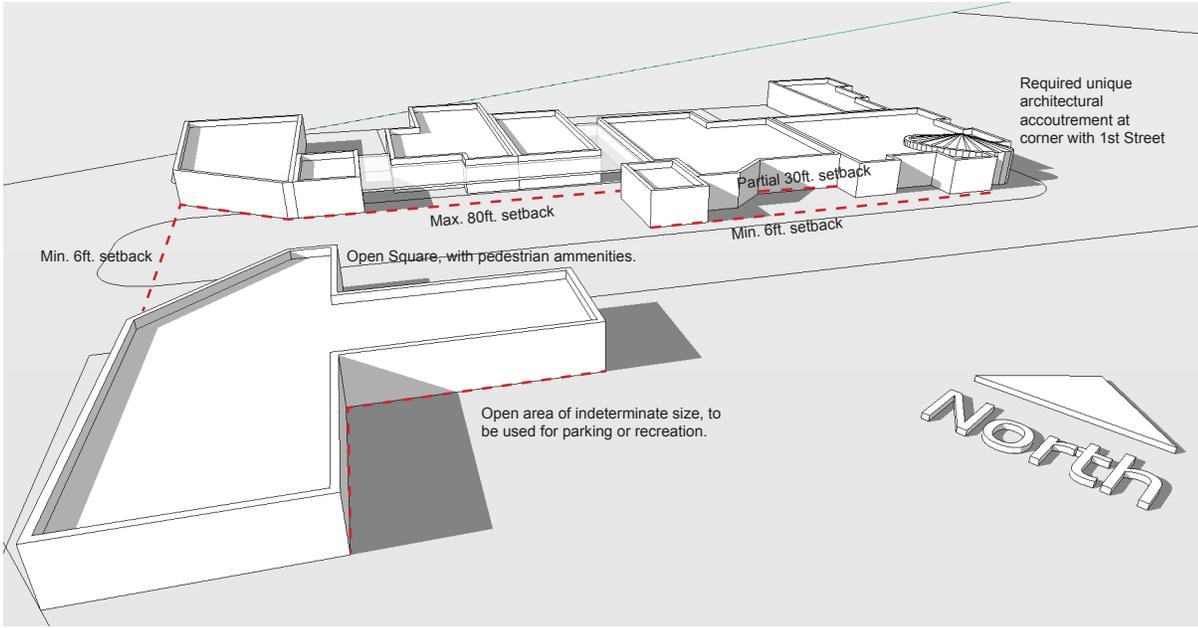
ALLOWED LAND USES	
Ground floor	retail/food service
Upper floor(s)	office/residential/clock tower
SETBACKS	
From street front on ground floor	0'-6' from property line
From street front on upper floor(s)	0'-6' from property line
From side property lines on ground floor	0'-6' from property line
From side property lines on upper floor(s)	0'-6' from property line
From back property line on ground floor	0'-6' from property line
From back property line on upper floor(s)	0'-6' from property line
BUILD-TO-LINES	
On street front on ground floor	~75% @ property line
On street front on upper floor(s)	~50% @ property line, 20% @ 6'
On side property lines on ground floor	~75% @ property line
On side property lines on upper floor(s)	~50% @ property line, 20% @ 6'
On back property line on ground floor	~75% @ property line
On back property line on upper floor(s)	~50% @ property line, 20% @ 6'
BUILDING HEIGHTS	
Ground floor (minimum and maximum)	10'-12'
Upper floor(s) (minimum and maximum)	9'-11'
FENESTRATION	
Required fenestration area (% total street facade) on ground floor	90%
Required fenestration area (% total street facade) on upper floor(s)	60%
PARKING REQUIREMENTS	
Number of required parking spots per sq. ft. of USE 1 (specify)	1/300 sq. ft.
SIGNAGE	
Allowed signage types	Projecting or Hanging
Allowed signage heights	8'-10'



Local/Family Commercial

Urban Design Objectives for the Regulation Area:

To provide a place for families, both local and regional, to come for a variety of fine cuisine. This area will help Buellton attract people as a destination to enjoy an afternoon in an aesthetically pleasing and comforting environment, rather than as a quick stop on Highway 101.



Local/Family Commercial

ALLOWED LAND USES

Ground floor	retail/food service
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SETBACKS

From street front on ground floor	Min. 6ft., or Max 80ft. from PR
From street front on upper floor(s)	6ft.-72ft. from PR
From side property lines on ground floor	0ft.-6t. from PR
From side property lines on upper floor(s)	0ft.-6ft. from PR
From back property line on ground floor	20ft.-100ft. from PR
From back property line on upper floor(s)	20ft.-100ft. from PR

BUILD-TO-LINES

On street front on ground floor	30% @15ft. from PR
On street front on upper floor(s)	30% @15ft. from PR
On side property lines on ground floor	30% @ PR
On side property lines on upper floor(s)	30% @ PR
On back property line on ground floor	30% @ 20ft., and 40% @ 100ft. from PR
On back property line on upper floor(s)	30% @ 20ft., and 40% @ 100ft. from PR

BUILDING HEIGHTS

Ground floor (minimum and maximum)	9ft.-12ft.
Upper floor(s) (minimum and maximum)	8ft.-10ft.

FENESTRATION

Required fenestration area (% total street facade) on ground floor	80%
Required fenestration area (% total street facade) on upper floor(s)	90%

PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of USE 1 (specify)	1/300 sq. ft.
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SIGNAGE

Allowed signage types	Projecting or Hanging
Allowed signage heights	8'-10'



Plaza/Medians

Urban Design Objectives for the Regulation Area:

The urban form for the plaza at Second Street and Avenue of Flags and for the medians to the North of the plaza are allowed to be versatile. However, they must adhere to the guidelines delineated below:

The Plaza

The Plaza is intended to host activities such as art fairs, wine tasting festivals, and farmers markets. Bands would also be welcome to play during the afternoons or evenings. The plaza should be a gathering space for the local business people, tourists, and pedestrians.

The Plaza must be paved with permeable pavers. The vegetation is limited to street trees, small, decorative trees, and vegetation in raised planters, or in low lying beds surrounding the base of trees. Trees should not interfere with the main gathering space where festivals will occur. Trees should be planted as a buffer between the streets and the plaza. A small fountain should be permitted on the plaza.

Commercial uses intended for the Plaza are small, semi-permanent restaurants or food/drink stands. Examples of these uses include but are not limited to: coffee kiosk; juice hut; ice cream stand; sandwich bar, beer and bratwurst restaurant. These uses are intended to be treated as permanent fixtures however mobile they could be. They should only be displaced to make space for recreation or entertainment on the plaza, however, it is advised to work around them.

The Median to the North of the Plaza

The median to the north of the Plaza should have to be aesthetically similar to the Plaza, to allow for continuity along the Avenue of Flags.

More flexibility is allowed in the placement of its trees and plants along the median. Some of the median is allowed to

have ground covering plants, preferably native, non-invasive and non-turf plantings.

There should be at least one small, semi-permanent commercial use on this median with outdoor seating facing the southern view. Shade structures or umbrellas must be provided for this seating.

There must be a clear paved pedestrian path which leads into a crosswalk and then leads into the Plaza.

Roundabout

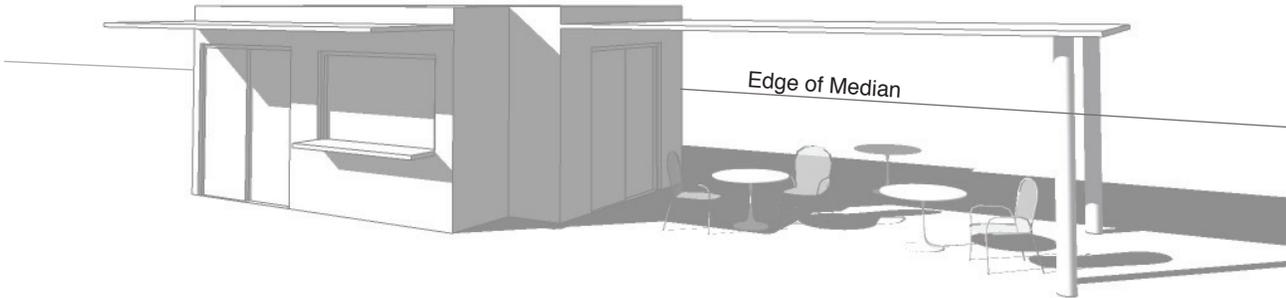
The roundabout should have interesting feature(s) on it. It is allowed to have ground covering plants, preferably native, non-invasive and non-turf plantings. The main interesting feature can be a tree, an art piece, flags, or a water feature.

Northernmost Median

The Northernmost median will have non-turf ground covering plantings.

It should have a 'Welcome to Downtown Buellton' sign and identifying features to be determined by the city.

Permeable paved crosswalks must extend from the Gas Station to the Hotel, through the median. There should be at least two 10' wide crosswalks cutting through the median. Signage to indicate the crosswalks locations should be placed in clear view of North and Southbound traffic. Flashing lights should turn on when a pedestrian is waiting to cross and while they are crossing.



Plaza/Medians | Building Siting and Massing

ALLOWED LAND USES

Ground floor	Semi-permanent, small scale commercial, Examples: Coffee kiosk made from shipping containers
Upper floor(s)	Not Permitted

Placement

Front facade must face median	All sides need 60-80% fenestration to prevent blank facades
Outdoor seating (if applicable)	Must be easily and safely accessible from two sides

BUILDING HEIGHTS

Ground floor (minimum and maximum)	8' Minimum, 10' Maximum
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BUILDING SIZE

Minimum and maximum square footage

FENESTRATION

Required fenestration area (% total street facade) on ground floor	60-80%
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PARKING REQUIREMENTS

Number of required parking spots per sq. ft. of USE 1 (specify)	None
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SIGNAGE

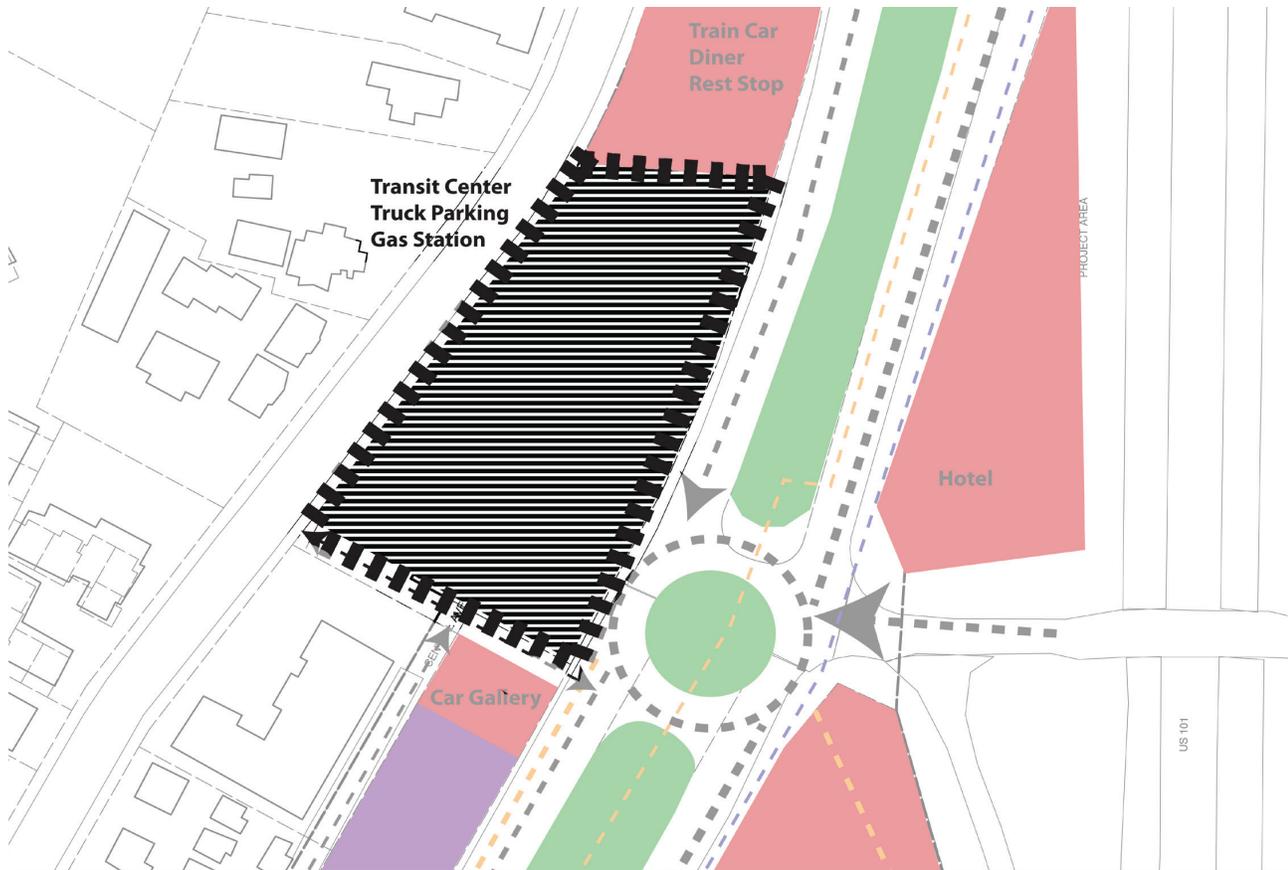
Allowed signage types	Surface or hanging/projected
Allowed signage heights	4' Minimum, 9' Maximum



Park and Creek

Urban Design Objectives for the Regulation Area:

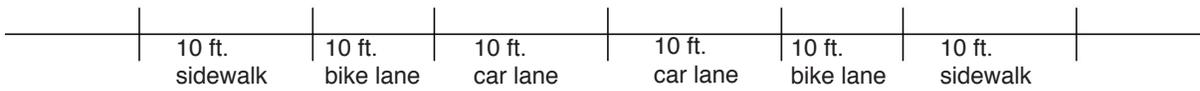
The park regulation area spans from Highway 246 to Damasa Road. The buildings on the adjacent parcels are built to the property line to create presence at the park. Through the several community meetings in Buellton, there has been an overwhelming request for an interactive outdoor nature area. In the proposed park there are several features that would answer the requests from the residents. Spanning the whole length of the area would be a walking/biking trail. The trail would connect with the Santa Ynez River. Along the trail there would be informational placards with information about the native plant and animal species and the history of Buellton. Just south of Second Street in the park, there could be an art park that would showcase local artists' sculptures. A little north of Second Street would be a children's interactive park.



Light Industrial

Urban Design Objectives for the Regulation Area:

The design objectives of this area is is to maintain the light industrial uses on the site. This would maintain the gas station that is currently on the parcel. The team is proposing to add truck parking, where it will provide covered parking for the trucks and give them a place to rest. The building for truck parking is also proposed to be a green roof that will provide low impact development strategies for the area. There would also be a transit center that will create and regulate the bus system that is proposed for the City of Buellton.



Highway 246 to First Street | Streetscaping

TRAFFIC LANES

Direction 1, number and width	North, 10'
Direction 2, number and width	South, 10'

BIKE LANES

Direction 1, width	North, 6'
Direction 2, width	South, 6'

SIDEWALKS

Direction 1, width	North, 10'
Direction 2, width	South, 10'

MEDIAN (IF AVAILABLE)

Width	none
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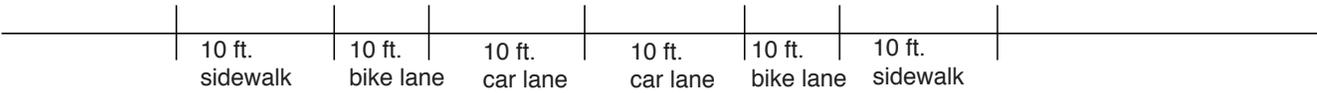
Other properties

STREET TREES

Direction 1, type	North and South, alternating 2 Australian Willow, 1 Chinese Pistache
Direction 1, frequency	Starting 5 feet North of Hwy 246, plant every 25-30'
Direction 2, type	North and South; Around entrance at 246 to Avenue of Flags Purple Leaf Plum
Direction 2, frequency	Plant every 20-25'

STREET LIGHTING

Type 1, purpose and frequency	Northbound, pedestrian oriented, 16 feet tall, every 50 feet
Type 2, purpose and frequency	Southbound, pedestrian oriented, 16 feet tall, every 50 feet



First to Second Street | Streetscaping

TRAFFIC LANES

Direction 1, number and width	Northbound, 10ft
Direction 2, number and width	Southbound, 10ft

BIKE LANES

Direction 1, width	Northbound, 6ft
Direction 2, width	Southbound, 6ft

SIDEWALKS

Direction 1, width	Westside, 10ft
Direction 2, width	Eastside, 10ft

MEDIAN (IF AVAILABLE)

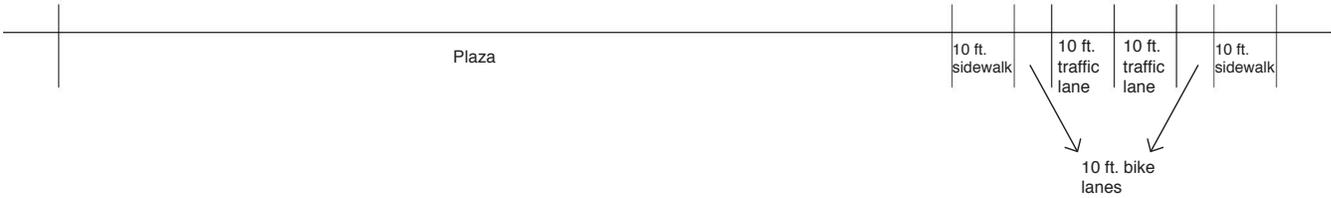
Width	N/A
Other properties	N/A

STREET TREES

Direction 1, type	Northbound, Australian Willow and Chinese Pistache
Direction 1, frequency	Every 100 ft, alternating 2 Australian Willow, 1 Chinese Pistache
Direction 2, type	Southbound, Australian Willow and Chinese Pistache
Direction 2, frequency	Every 100 ft, alternating 2 Australian Willow, 1 Chinese Pistache

STREET LIGHTING

Type 1, purpose and frequency	Northbound, pedestrian oriented, 16 feet tall, every 50 feet
Type 2, purpose and frequency	Southbound, pedestrian oriented, 16 feet tall, every 50 feet



Second Street to Vintage Walk | Streetscaping

TRAFFIC LANES	
Direction 1, number and width	Northbound, 1 lane, 11 ft.
Direction 2, number and width	Southbound, 1 lane, 11 ft.
BIKE LANES	
Direction 1, width	Northbound, 6 ft.
Direction 2, width	Southbound, 6 ft.
SIDEWALKS	
Direction 1, width	Northbound, 10 ft.
Direction 2, width	Southbound converted median, 143 ft.
MEDIAN (IF AVAILABLE)	
Width	None
Other properties	N/A
STREET TREES	
Direction 1, type	Northbound, Australian Willow and Chinese Pistache
Direction 1, frequency	Every 100 feet, alternation every 2 Australian Willows for 1 Chinese Pistache
Direction 2, type	Southbound, Australian Willow and Chinese Pistache
Direction 2, frequency	Every 100 feet, alternation every 2 Australian Willows for 1 Chinese Pistache
STREET LIGHTING	
Type 1, purpose and frequency	Northbound, pedestrian oriented, 16 ft. tall, every 50 ft.
Type 2, purpose and frequency	Southbound, pedestrian oriented, 16 ft. tall, every 50 ft.



Damassa Road to Hwy 101 | Streetscaping

TRAFFIC LANES

Direction 1, number and width	1 lane, 11'
Direction 2, number and width	1 lane, 11'

BIKE LANES

Direction 1, width	6'
Direction 2, width	6'

SIDEWALKS

Direction 1, width	30'-35'
Direction 2, width	30'-35'

MEDIAN (IF AVAILABLE)

Width	70'-80'
Other properties	

STREET TREES

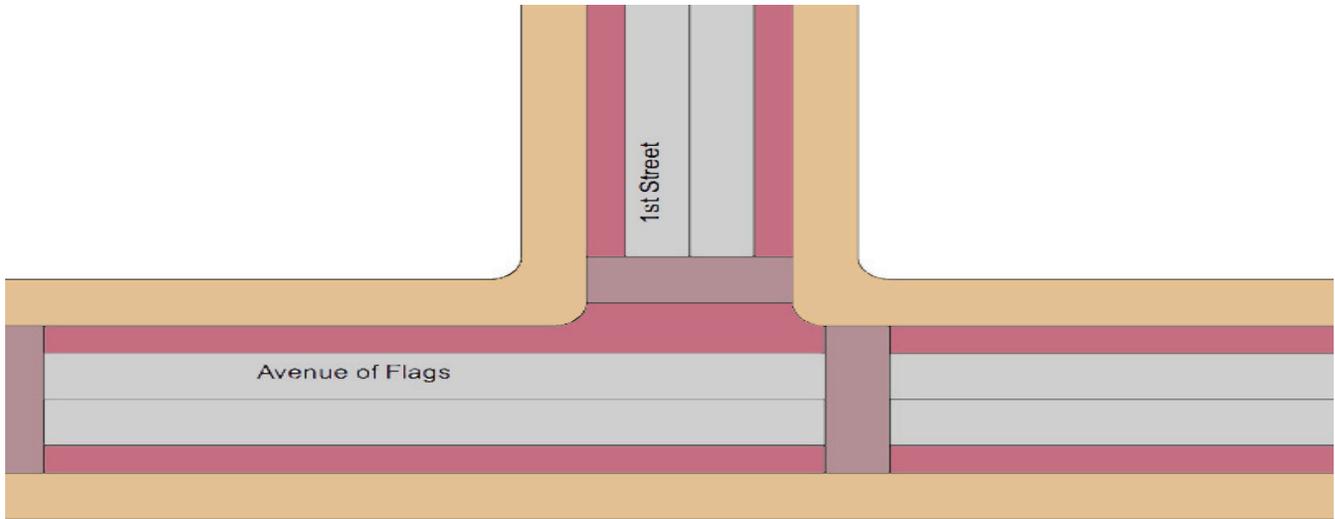
Direction 1, type	various
Direction 1, frequency	30'
Direction 2, type	various
Direction 2, frequency	30'

STREET LIGHTING

Type 1, purpose and frequency	pedestrian, low-level vehicular; 20'
Type 2, purpose and frequency	pedestrian, low-level vehicular; 20'

RECOMMENDED LOW-IMPACT DEVELOPMENT STRATEGIES

Technique	Explanation
permeable asphalt	asphalt that allows drainage of water



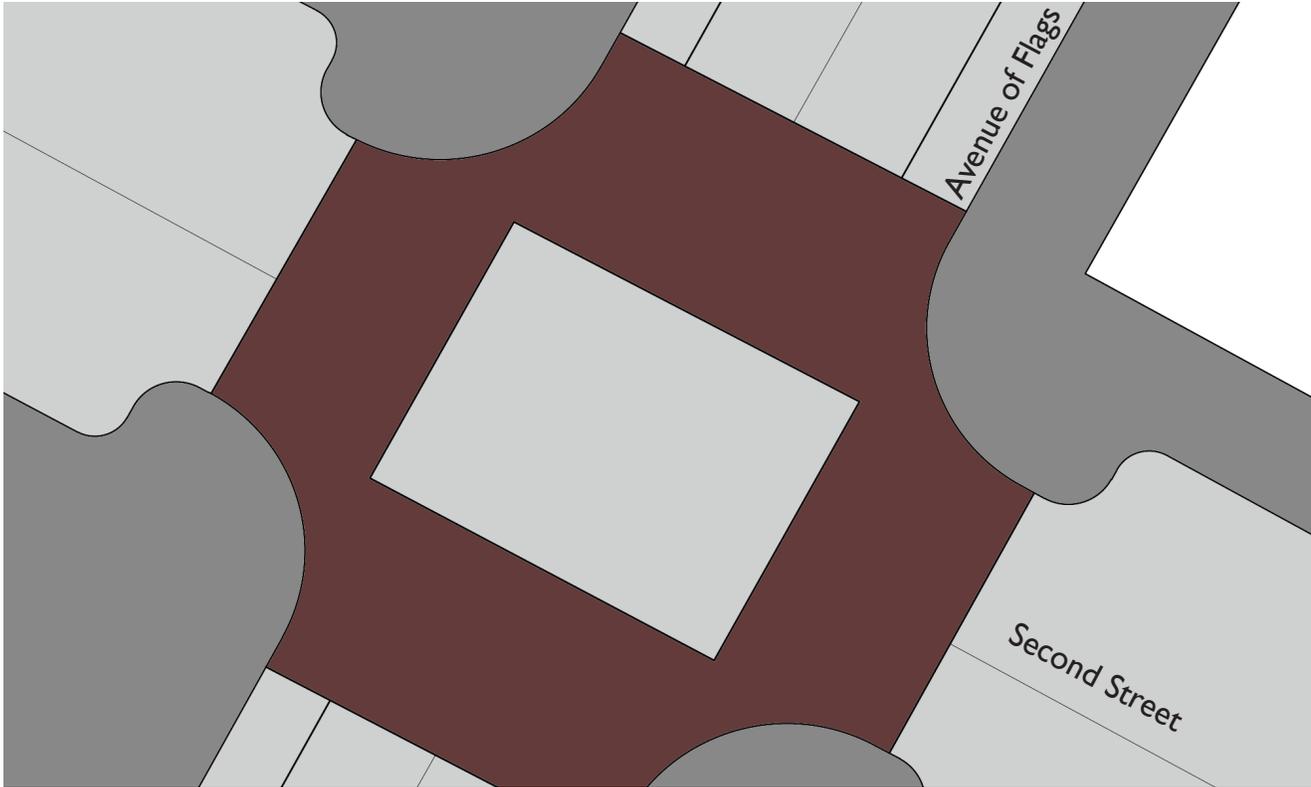
First Street and Avenue of Flags

TRAFFIC CALMING MEASURE(S)

Traffic calming type	Raised, permeable crosswalks
	Street furniture and trees
	Close building presence

PEDESTRIAN CROSSINGS

Direction:	North-South on 1st
Width	10'
Material(s)	Permeable pavers, brick and or other tan/warm color
Direction:	East-West; 20' South of 1st, crossing Avenue of Flags
Width	10'
Material(s)	Permeable pavers, brick and or other tan/warm color
Direction:	East-West; North of 1st, crossing Avenue of Flags and connecting to pathway linking Zaca Creek Open Space to Avenue of Flags
Width	10'
Material(s)	Permeable pavers, brick and or other tan/warm color



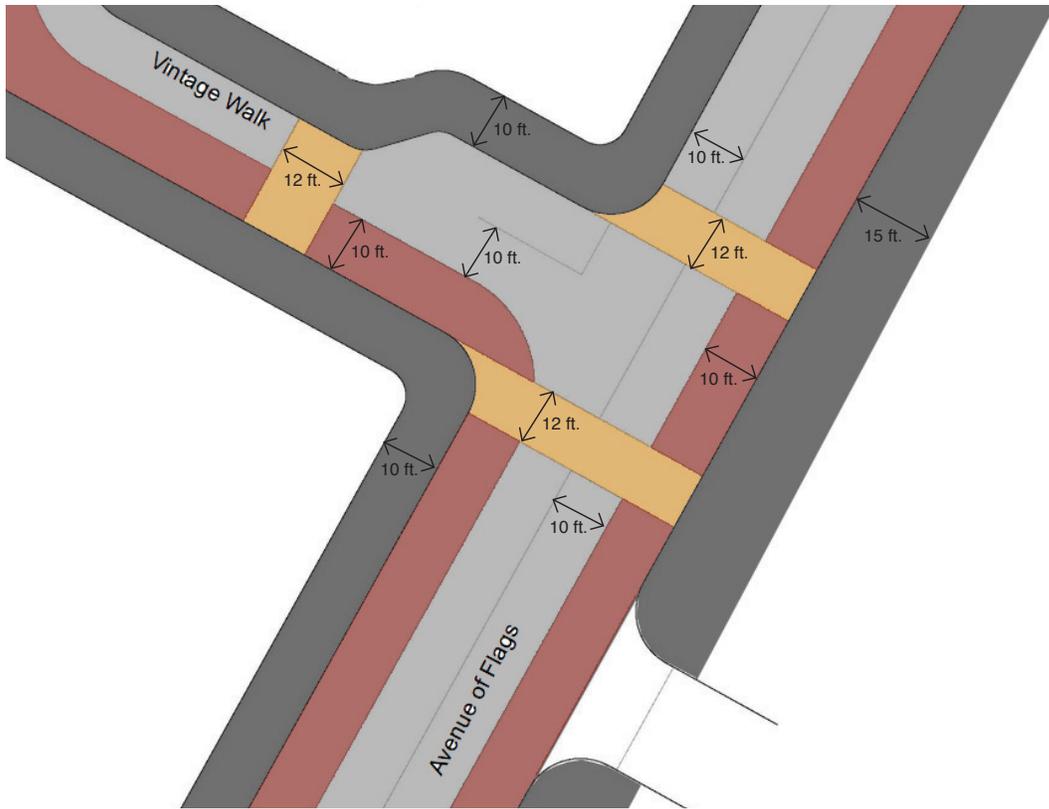
Intersection Second Street and Avenue of Flags

TRAFFIC CALMING MEASURE(S)

Traffic calming type	Bulb outs on two corners to calm traffic, maximize pedestrian safety and provide side parking on second street

PEDESTRIAN CROSSINGS

Direction:	Northbound
Width	15ft
Material(s)	Permeable pavers, brick and or other tan/warm color
Direction:	Southbound
Width	15ft
Material(s)	Permeable pavers, brick and or other tan/warm color
Direction:	Eastbound
Width	15ft
Material(s)	Permeable pavers, brick and or other tan/warm color
Direction:	Westbound
Width	15ft
Material(s)	Permeable pavers, brick and or other tan/warm color



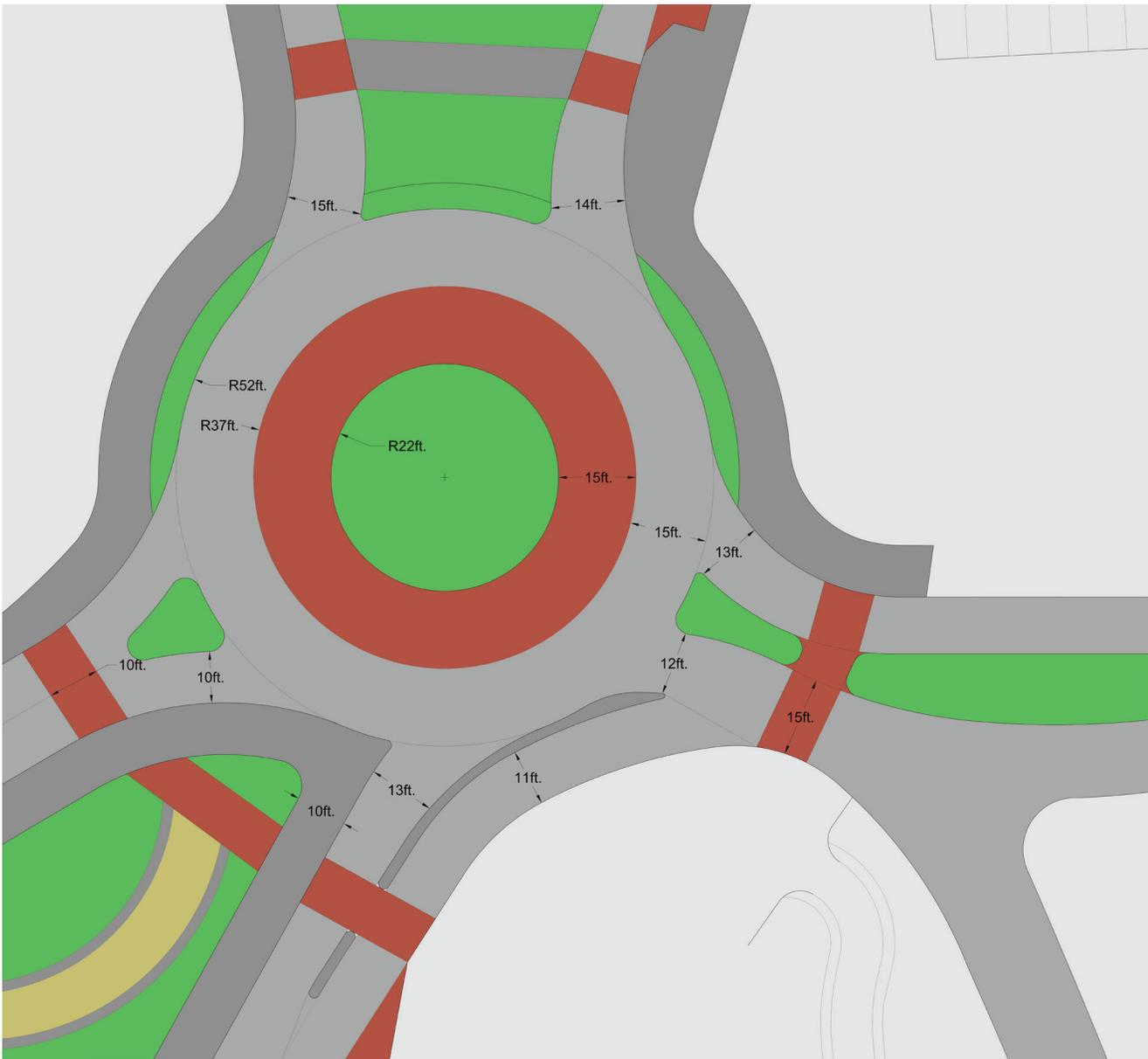
Vintage Walk and Avenue of Flags

TRAFFIC CALMING MEASURE(S)

Traffic calming type	Raised, permeable crosswalks
	Close building presence

PEDESTRIAN CROSSINGS

Direction:	North-South on Vintage Walk
Width	12 ft.
Material(s)	Permeable pavers, brick and/or other tan/warm color
Direction:	East-West, north end
Width	12 ft.
Material(s)	Permeable pavers, brick and/or other tan/warm color
Direction:	East-West, south end
Width	12 ft.
Material(s)	Permeable pavers, brick and/or other tan/warm color



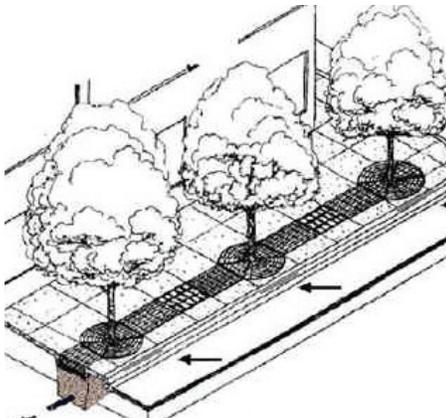
Intersection **Damassa Road and Avenue of Flags**

TRAFFIC CALMING MEASURE(S)

Roundabout	52' radius
	3 directions
	landscaped center island with mountable median

PEDESTRIAN CROSSINGS

Width	10'
Material(s)	raised brick/keystone pavers
Notes:	pedestrian crossings at all points of entry/egress
	placed 25' from outer circumference of roundabout



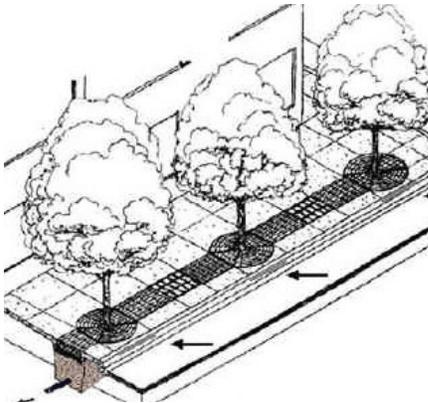
Public Property | Low-impact Development Guidelines

STRUCTURES: RECOMMENDED LOW-IMPACT DEVELOPMENT STRATEGIES

Technique	Explanation
Green Roofs	For new structures or old structures that are strong enough to hold a green roof. The installation of a green roof would absorb rain water and provide insulation. This would also help mitigate the heat island effect.
Rain Barrels	The installation of rain barrels would allow run off water to be collected and used for later rather than it going into the storm drain

LANDSCAPING: RECOMMENDED LOW-IMPACT DEVELOPMENT STRATEGIES

Pervious Concrete	The use of pervious concrete instead of asphalt would allow rain water to be absorbed into the ground and reducing the run off. This would mainly be used as pedestrian walkways and plaza areas.
Bioswale	The installation of bioswales would remove silt and pollution from the run off water before it enters the storm drain.
Tree Box Filters	The use of tree box filters would allow the storage of rain water under the sidewalks. This would prevent more run off.



Private Property | Low-impact Development Guidelines

STRUCTURES: RECOMMENDED LOW-IMPACT DEVELOPMENT STRATEGIES

Technique	Explanation
Green Roofs	For new structures or old structures that are strong enough to hold a green roof. The installation of a green roof would absorb rain water and provide insulation. This would also help mitigate the heat island effect.
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Illustrative Site Plan





5 | Form-Based Codes









5 | Form-Based Codes







5 | Form-Based Codes

Photos

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